



57-65 Dale Street, Liverpool, Merseyside

Watching Brief Report



Oxford Archaeology North

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**Falconer Chester Hall and
John Sisk and Co Ltd**

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SUMMARY

A watching brief was undertaken by Oxford Archaeology North on the site of the former buildings at 57/59 Dale Street (Eagle Star House), 61/63 Dale Street, 65/67 Dale Street (Pioneer Building) (Plate 1) and 10/12 Vernon Street, Liverpool, in preparation for the construction of a new hotel, associated retail and car parking (NGR SJ 34420 90640). The works were undertaken following the demolition of the buildings, but as these all had deep cellars there was little potential for the survival of buried archaeological remains. The principal area of investigation was an area of land, previously used as car parking for 10/12 Vernon Street, which had potential for the preservation of earlier surviving remains, specifically those that predate the realignment of Dale Street in the 1860s. As such, a watching brief was carried out within the areas to be affected by the ground works in order to examine any phases of construction within the site boundary which came to light as a result of the demolition works. The watching brief was undertaken in phases across five closely located areas.

All of the archaeological structures and deposits identified during the course of the watching brief were sealed beneath cobble-stone setts of the pre-Second World War alignment of Hale Street. This road, which was contemporary with the remodelling and realignment of Dale Street during the 1860s, indicating that all of the surviving deposits below it predate the major Victorian remodelling of the business district.

A series of surfaces and brick foundations was identified which represent the remains of small warehouses and ancillary structures that were demolished in order to facilitate the expansion of Dale Street. This was done in order to create the broader Victorian thoroughfare which still remains today, with the majority of the buildings to the east of the Town Hall being constructed after 1860.

The earliest archaeological feature was a truncated curvilinear ditch **013**, in Area 2, which was found to contain a dark organic material **014**, and contained mid-eighteenth century pottery. The ditch was possibly a drainage or boundary and is all that remains of the early post-medieval landscape within this area of Dale Street as the rest of the development footprint had been truncated by extensive cellar construction undertaken after 1860.

The most complete structure identified during the course of the excavation was a handmade brick-lined well shaft, with surrounding plinth and an intact domed brick capping. The well was 25m deep and dry, and empty of backfill or detritus.

Historic images, such as the view of Hale Street and Vernon Street from Cheapside (Plate 1), demonstrate the organic and cramped nature in which sheds, lean-to buildings and warehouses had developed within the study area along the northern side of Dale Street. Not all of these structures would have had substantial foundations and it is likely that much of this earlier activity has been severely truncated and now only survives in ever decreasing pockets of ground between larger buildings and beneath earlier roadways and footpaths.

ACKNOWLEDGEMENTS

Oxford Archaeology North (OA North) would like to thank John Sisk and Co and, in particular Roger LeBlanc and Ian Sammonds for facilitating the project,. We would also like to thank Philippa Parker of Falconer Chester Hall, for her support at the outset of the project. We would also like to thank Anne-Marie Piedot and John Hinchliffe, of Liverpool City Council for providing planning advice.

Thanks to the site staff and groundworkers of John Sisk & Son Ltd, including Site Manager Conor Keating. Special thanks go to all who assisted on site, including the staff and machine operators of Ashworth Demolition.

The watching brief was undertaken by Aidan Parker and Caroline Raynor. The report was written by Caroline Raynor and the drawings were produced by Anne Stewardson. The pottery was examined by Jeremy Bradley. The project was managed by Jamie Quartermaine, who also edited the report.

1. INTRODUCTION

1.1 CIRCUMSTANCES OF PROJECT

- 1.1.1 An investigation of four buildings; 57/59 Dale Street (Eagle Star House), 61/63 Dale Street, 65/67 Dale Street (Pioneer Building) (Plate 1) and 10/12 Vernon Street was undertaken by Oxford Archaeology North (OA North 2011) at the request of Falconer Chester Hall. The work was required to provide a mitigative record of the buildings in advance of the complete demolition of 61/63 Dale Street and 10/12 Vernon Street and the partial demolition of 57/59 Dale Street, as well as significant alteration to 65/67 Dale Street, as part a larger redevelopment to create a new city centre hotel on the site. None of the buildings were listed, but the site is within the Castle Street Conservation Area and is within the Liverpool Maritime Mercantile World Heritage Site (Fig 1 and 2). An assessment of significance of the structures, in the form of a PPS5 statement, has been compiled by Peter de Figueiredo which has served to inform the design of the new build. This also included an outline documentary study which assessed the archaeological and historical significance of the site, and highlighted that the buildings have a broad range of fabric reflecting the development of the site from the late nineteenth century.
- 1.1.2 A building survey (OA North 2011) was undertaken of the structures in advance of demolition, which was undertaken in accordance with a project design prepared by OA North (*Appendix 1*). After the completion of the building survey, the complete demolition of the 10/12 Vernon Street, 61/63 Dale Street and the partial demolition of both 57/59 Dale Street (Eagle Star House) and 65/67 Dale Street (Pioneer Building) was carried out in preparation for the construction of a new hotel and associated retail and car parking. Within the core demolition area, there was only a very limited chance of surviving archaeological remains, as all the buildings fronting onto Dale Street were constructed with extensive cellars. To the rear of the demolished buildings an area of land, previously used as car parking for 10/12 Vernon Street, was identified as being a site of archaeological interest with potential for the preservation of earlier surviving remains, specifically those that predate the realignment of Dale Street in the 1860s. As such a watching brief was carried out within the areas to be affected by the new pile wall in accordance with the specifications of the project design (*Appendix 1*).

1.2 LOCATION, TOPOGRAPHY AND GEOLOGY

- 1.2.1 The site under investigation is located at NGR SJ 34420 90640, and occupies approximately 1523m² (Fig 1 and 2). The former 57-67 Dale Street block was located on the north side of Dale Street, towards the eastern end of the World Heritage Site conservation area; 10/12 Hale Street was located to the rear of 61/63 Dale Street on Hale Street (Fig 2). The site is bounded to the north by Hale Street and a one way access to a multi-storey car park, to the east by Vernon Street, to the south by Dale Street and to the west by a narrow pedestrian footpath which is recognised as an element of Hale Street. Dale Street is located within the Commercial District of Liverpool City Centre and is an area characterised by numerous historic buildings, fifteen of which are listed because of their historic and architectural significance (de Figueiredo 2010).

1.2.2 The underlying geology of the site comprises Helsby Sandstone Formation Sandstone (British Geological Survey 1:625,000).

2. METHODOLOGY

2.1 PROJECT DESIGN

- 2.1.1 A project design was submitted by OA North (*Appendix 1*) for a building survey and watching brief, in response to a verbal brief by the former Merseyside Archaeologist and in accordance with the defined planning conditions (application no 09F/2531), which, in terms of the watching brief, stipulated that: *the developer shall, until the completion of the development afford access at all reasonable times to any archaeologist nominated by the local planning authority and shall allow the observation of the excavations and the recording of items of interest and finds.*
- 2.1.2 The project design provided the basis for maintaining an archaeological watching brief during any intrusive ground works, and to record any archaeological remains revealed. It was adhered to in full, and the work was consistent with the relevant standards and procedures of the Institute of Field Archaeologists, and generally accepted best practice.

2.2 OBJECTIVES

- 2.2.1 Previous surveys and documentary studies (OA North 2011) have demonstrated that along this part of Dale Street there is considerable potential for archaeological deposits and structures to survive from the early post-medieval and/ or earlier periods.
- 2.2.2 The objectives of the watching brief are as defined in the Project Design (*Appendix 1*) and these are:
- To record accurately any surviving archaeological features or deposits by means of detailed observation and recording. To record the presence of buried features by appropriate recovery techniques, where applicable;
 - In the event of discovering significant features, a programme of rapid excavation will record the features within a wider context to allow an appropriate understanding of the remains;
 - A full written report will assess the significance of the data generated by the entire programme of work, in a local and regional context, and will be suitable for deposition as a permanent archive of the work undertaken.
- 2.2.3 To these ends it was necessary to assess the thickness, depth and depositional history of any significant archaeological structures and/or deposits. The depth of investigation was limited by the depth of ground works and health and safety limitations.

2.3 WATCHING BRIEF

- 2.3.1 Following the completion of the building investigation, an intermittent watching brief was maintained throughout the course of the demolition and subsequent works in accordance with the project design (*Appendix 1*); this served to identify materials or features relating to the Dale Street buildings and possible earlier features which had previously been obscured by modern additions and alterations. This work was carried out in conjunction with demolition works undertaken by Ashworths Demolition and groundworks undertaken by John Sisk and Son Ltd, and all work was undertaken following consultation with the site foreman, Conor Keating.
- 2.3.2 A permanent archaeological presence was maintained during groundworks in conjunction with the construction work carried out by the principal contractors John Sisks and Son Ltd. The purpose of this watching brief was to identify, investigate and record any archaeological remains encountered.
- 2.3.3 A daily record of the nature, extent and depths of groundworks was maintained throughout the duration of the project. All archaeological contexts were recorded on OA North's *pro-forma* sheets, using a system based on that of the English Heritage former Centre for Archaeology. A monochrome and digital photographic record was maintained throughout.

2.4 ARCHIVE

- 2.4.1 A full professional archive has been compiled in accordance with current IFA and English Heritage guidelines (English Heritage 2006). The paper and digital archive will be deposited with the Merseyside Record Office on completion of the project

3. HISTORICAL AND ARCHAEOLOGICAL BACKGROUND

3.1 PRE-EIGHTEENTH CENTURY

- 3.1.1 Dale Street is recognised as being one of the earliest streets to develop in the town of *Liuerpul* during the medieval period and forms part of the original 'H-shaped' street plan which extended from the foreshore to the head of the pool (at Shaw's Brow). These streets, present at the time Liverpool was included in the 1207 King John Charter, included: Castle Street; Chapel Street; Dale Street; Juggler (or Jongler) Street; Old Hall (or Milne Street); More Street and Water Street (Belchem 2006, 72). Of these, Dale Street, Water Street and Chapel Street still exist under the same name today.
- 3.1.2 Dale Street allegedly takes its name from the dale or open fields to which it led. This dale was located at the east end of the street and was accessible via a stone bridge (stony bridge or Townsend Bridge) which, crossed the neck of the pool near the Gallows Field and the Great Heath (Horton 2002, 1), now the site of some of Liverpool's most superior neo-classical buildings, including St George's Hall and the William Brown Library.
- 3.1.3 By the end of the Tudor Period (1485-1603), little had occurred to further the development or expansion of these streets; however, documentary evidence does suggest that by this time Dale Street had been subject to improvements. Historian John Leland asserts that Liverpool was a '*pavid towne*' with evidence for a paver from Warrington attending to half of Dale Street in one year and the other half in the following year (Belchem 2006, 72).
- 3.1.4 Dale Street is mentioned by James Wallace in respect to its part in the civil war siege; '*We are expressly told in the memoirs that the town was enclosed from the end of Dale Street and the length of the town from the west end of Water Street to the east end of Dale Street, the corner of Byrom Street is but nine-hundred yards...*' (Wallace 1795, 50).
- 3.1.5 Okill's Map of 1650 (Fig 3) indicates that the plot of land now occupied by 57-67 Dale Street was already partially occupied by a number of small buildings, all of which fronted onto Dale Street. It is unlikely that these structures were anything more significant than terraced, timber-framed dwellings. Okill's Map shows that the rear of these buildings faced onto open fields (the map does not provide any information relating to the owner of these plots). The fields nearest to the site were divided into three substantial plots; two square plots directly north of the site and one longer plot orientated north/south on its long axis, the western boundary of which is consistent with the line of the current Vernon Street.
- 3.1.6 At this time, the map suggests that virtually the entire area on the north side of Dale Street was then populated with dwellings, but the south-east side of Dale Street was more sparsely populated with a number of open plots, including those owned by the Earl of Derby and Mr Crosse Esquire.

3.2 EIGHTEENTH CENTURY DALE STREET

- 3.2.1 John Eyes' Map of 1765 (Fig 4) demonstrates the comparatively rapid expansion of urban Liverpool, the majority of which was spurred on by the opening of Thomas

Steer's Wet Dock at the mouth of the Pool in 1715. This was followed, in fairly rapid succession, by the expansion of the dock network. The creation of this advanced system of inland waterways, closed off from the river, not only provided work for a migrant population of masons, brick makers and labourers, but also encouraged more permanent settlement by merchants, ship builders, and ship owners.

- 3.2.2 By 1765 all the plots of land on the north and south sides of Dale Street were occupied, in so much as there were no more available frontages. The spaces behind the buildings continued to exist as open land, although presumably they were, by then, given over to storage and open-air work space rather than arable land. At this time Vernon Street appeared on the map, although it is clear that it had been re-aligned over the years and now no longer occupies its original course within the town layout. Located to the south-east of Vernon Street is an area marked as a large garden or possibly an orchard. To the west of the site, Hale Street is yet to appear and instead there were two alleys, known as *Bachelor's Lane* and *Glass House Wient*. Although neither of these small streets were likely to have been salubrious (the name Bachelor's Lane implies that the street may have been associated with prostitution), both have entries in the 1766 Gore's Directory (a popular street directory listing the names, professions and addresses of people living in the town). *Glass House Wient* was home to Isaac Johnson, a hosier and the Reverend John Johnson. The two men listed at *Bachelor's Lane* were John Coleman, a bread-baker, and John Grimshaw, a butcher. The presence of their professions supports the idea that this area of Dale Street continued to be occupied predominantly by the working or labouring classes.
- 3.2.3 By 1766 Dale Street was a flourishing and traditional location and is the most populous street in the town at this time with sixty-nine persons being listed as living and/or having a business premises there. It is interesting to note that Castle Street and Water Street (also two of the oldest streets) are the next most populous, having fifty-two and forty-two persons listed respectively.
- 3.2.4 At this time Dale Street was occupied by a mixed cross-section of the population; working class grocers, chair-makers, saddlers, mercers, cobblers and smiths rubbed shoulders with inn keepers, members of the clergy, surgeons and merchants. The more affluent north-eastern end of Dale Street was occupied by people in public office, including William Stringfellow, Overseer of the Poor, and the Alderman Thomas Shaw who owned the successful potteries on Shaw's Brow (Gore 1766, 56). It should also be noted that John Gore, who was responsible for collecting and publishing the data in the eponymous directory, chose to establish his business premises on Dale Street, where he was listed as a bookseller and stationery salesman.
- 3.2.8 Liverpool was an expanding port town and attracted a high percentage of its population through migration. This would have helped shape the distinctive occupational, social and cultural structure of the city (Belchem 2006, 175). The late eighteenth century Liverpool economy was dominated by people working in transport related jobs, particularly mariners, as well as an increasing number of people working in manufacturing and craft industries. These were related not only to dockside requirements but also included clothing, food and potteries. This demographic, as highlighted by the statistics relating to Dale Street (above), throws into stark relief the dearth of people working in professional trades (*ibid*).

- 3.2.9 The high frequency of public houses on Dale Street, and their significance as meeting places and places of departure, is described by Richard Brooke in 1853. *'In 1775 the principal Inns in Liverpool were the Golden Lion, kept by Mr James Wrigley; The Fleece (or Golden Fleece) kept by Mr Thomas Banner; The Cross Keys kept by Mr John King; The Angel (sometimes called the Angel and Crown or the New Angel) kept by John Wilson; a further pub named The Angel kept by Mr John Phithian; The Bull and Punchbowl kept by Mr John Randles; The Woolpack kept by Mrs Woods and The Red Lion kept by Mr Ralph Hoult'* (Brooke 1853, 529).
- 3.2.10 Public Houses on Dale Street became important meeting places as carters and carriers announced their days of arrival and departure from these venues and then assembled to procure goods and persons for transport to numerous destinations across the north, including Derbyshire, Lancashire, Cumberland and Manchester. This steady procession of people entering and leaving the town would also have acted as an encouragement for the establishment of many small businesses. The wagons and carts were then mustered on the street until sufficient numbers had gathered before departing en masse as protection from the predations of the highway men who operated outside the towns and cities.
- 3.2.11 In the year 1780, Samuel Curwen, a leading American merchant from Salem visited Liverpool on his way to tour part of Britain and Europe. He travelled from Warrington to Prescot and from there would have entered the town via the Great Heath and Dale Street. He was hugely impressed by the size, scale and intricacies of the docks; however, he saw little to love in the town itself stating *'Houses are by great majority in middling and lower style, few, very few, rise above that mark. Streets long narrow and crooked, and dirty in an eminent degree.'* (Seed 2008, 2).
- 3.2.12 Charles Eyes' map of 1785 (Fig 5) further illustrates the expansion of the town and the steadily increasing density of public and private buildings around Dale Street and the original seven medieval streets. By this time, the H-shaped street plan has almost entirely been absorbed into the surrounding urban sprawl and was no longer quite so apparent. The site of 57-67 Dale Street was occupied and bounded to the west by Hale Street, which by then had replaced Glass House Wient as the principal alley. Hale Street extended northwards towards Old Hall Street, where it converged with Orange Street (now abolished). Vernon Street was present and follows its current alignment.
- 3.2.13 In 1786, the Corporation decided to review the town plan and adopt measures for the widening of some of the streets nearer to the Exchange. This may have been prompted by the rather organic network of streets and alleys around Dale Street, Water Street, but also by the earlier occurrence, in 1775, of riots in the area of the Exchange. These had proved difficult to control, partly due to the close confines of many of the buildings (Brooke 1853, 326; 385).
- 3.2.14 To further the widening and improving of streets around the Exchange, the Mayoralty petitioned to London to be allowed to make improvements to the town, which was approved and the Improvement Act 26.GeoIII. 12 passed (Brooke 1853, 385). Under the Act, Castle Street and parts of Water Street, Dale Street, Fenwick Street, Derby Square (former site of the Castle) and Preeson's Row were widened and improved.
- 3.2.15 Wallace (Wallace 1795, 78) suggests that by this late eighteenth century date, Dale Street was beginning to establish itself as a more hospitable and upmarket locale,

separate from the areas of Mann Island, Nova Scotia and other districts more directly linked to the dockside activities and those who worked at the docks. This contrasts directly with the fact that for a number of years, Dale Street was mostly noted as being the site of the earliest alms houses in the city, although these were demolished in 1748 (*op cit*, 162). The 1795 Map (source unknown) shows that the site was fully occupied, however the property is drawn out as one large block and individual buildings have not been defined. Gage's 1807 Map of Liverpool (Fig 6), shows Dale Street narrowing towards the east side, with the area to the west of Cumberland Street already having benefited from the widening and renovations which took place in the last quarter of the eighteenth century.

3.3 NINETEENTH CENTURY

- 3.3.1 *'It was not until the beginning of the nineteenth century, however that the old medieval core began to emerge as the town's dominant business district'* (Sharples and Stonard 2008, 8). The catalyst for a centrally-focused business district was the alterations to the Exchange, with new accommodation being provided for business transactions in the new Exchange Buildings, which now surround Exchange Flags to the rear of the Town Hall.
- 3.3.2 In 1820 a large swathe of the north side of Dale Street was taken down to further the widening of the street, with the frontage line being set back from the previous line of the road. Rebuilding was swift to recommence with development beginning at the western end around the Exchange and Town Hall (de Figueiredo 2010, 3).
- 3.3.3 Pigot's Directory of 1828-9 which deals with numerous counties including (but not limited to) Cheshire, Lancashire, Cumberland and Northumberland, makes mention of Dale Street and notes that *'Dale Street, Water Street and Tithebarn Street all exemplify the utility of, and services which the improvement act has and will ultimately produce.'* (Pigot 1828, 273).
- 3.3.4 The 1829 map, taken from *The Stranger in Liverpool* (Kaye 1812), provides evidence of more detail on the site and indicates the presence of at least four buildings occupying the plot, one of which is likely to be the structure now known as the Vernon Arms Public House. A gap between 67 and 69 Dale Street is likely to be the early formation of Nixon's Place, a narrow lane and courtyard, which was probably left to provide dray access. This map was produced after the north side of Dale Street had been taken down and the frontages moved to widen the road. The line of street in relation to the front of the Exchange and Town Hall has by this date clearly been altered.
- 3.3.5 *'In the 1850s and 1860s, the Exchange and the area around it became a magnet for speculative office developments and the headquarters for financial institutions. Property prices in its vicinity rose to levels that attracted national attention and the surrounding streets were almost entirely re-built'* (Sharples and Stonard 2008, 10). The 1851 OS Map (Fig 7) shows that the site is now densely populated with buildings laid out in much the same format seen as that shown on the 1863 Dower map (Fig 8) and in greater detail on the 1888 Goad Map (de Figueiredo, 2010). The boundaries of Vernon Street, Hale Street and Orange Street, as well as Nixon's Place, are clearly defined by this date.
- 3.3.6 In 1856 all of the houses in the borough of Liverpool were renumbered on a uniform system. Baines, in his book *Liverpool in 1859*, notes that *'Taking the town*

hall as a starting point, the line of Dale Street, London Road and Kensington (a line running east and west) was made the division between the northern and southern parts of the borough. The numbers of the streets running east and west commenced at the river or west end.' (Baines 1859, 101). Baines adds that the alterations to the numbering system involved the painting or affixing of 40,538 new numbers. This added to the cost of preparing lists of alterations for the post office and other public bodies and costed a total of £875.5s.9d.

- 3.3.7 The 1863 Dower Map (Fig 8) of Liverpool shows that the building then known as the Saddle Inn, now the Vernon Arms, and Nixon's Place remained unchanged. A larger gap had appeared at the rear of 65/67 Dale Street suggesting that perhaps the area had been cleared prior to a new spate of development on the plot. The Gore's Directory for 1867 provides a detailed list of everyone living or working in numbers 57 -65 Dale Street ; it should be noted, however, that at this time Hale Street was obviously not sufficiently populous or significant to warrant an entry into the directory and was omitted entirely.
- 3.3.8 Persons listed as living at 57 Dale Street, in the building known as Graecian Chambers (on the site now occupied by the frontage of the Eagle Star House building), included: Duncan George, commissions agent; Bankes and McGowan, attorneys; Richard Holden, attorneys; Worship, Starling and Day, attorneys; John Drew Clifford, a solicitor and Miller and Co tea and coffee dealers. Graecian Chambers was clearly a building designed to accommodate educated professionals; however, the directory indicates that the rest of the block facing onto Dale Street was still occupied by people involved in general trades and manufacture. These included at 59 Dale Street, James Cowan, Hatter; at 61 Dale Street, Kipling and Barrow, Hide Factors; at 63 Dale Street, Mr John Scragg, a wine and spirit merchant; at number 65, Samuel Millington, a butcher and at number 67, George Jordan, a musician (Gore 1867, 610).
- 3.3.9 The Gore's Directory for 1868 shows that there had been relatively little in the way of change to the function of the Graecian Chambers Building, which is not surprising given that it was purpose-built to fulfil a demand for clerks offices. The premises at number 61 Dale Street, however, had by that date been subdivided to provide accommodation for a number of additional businesses including: Thomas Dodd Jnr, Glass Merchant; Arthur Miller, Hide and Leather Factor; The Crown Life Assurance Company (with Arthur Miller acting as agent); Isaac Walker, a wool broker; William W. Emery, a corn and seed merchant and Abraham W. Emery, a wool stapler. The latter three occupations are a stark reminder that although Dale Street represented a functioning part of the commercial district, it was also still one of the main thoroughfares between the countryside and the docks and, as such, would still have seen the traffic of live animals and unprocessed raw materials as they moved to and from the ships.
- 3.3.10 By 1870, the Gore's Directory included a series of entries for Hale Street and Graecian Chambers was transferred from the Dale Street listing to the Hale Street listing. Graecian Chambers was by this date listed as being number 2 Hale Street and was still predominantly occupied by those engaged in professional trades, with the addition of Danson and Davies Architects into suite number 5.
- 3.3.11 Properties adjoining Graecian Chambers seem to have comprised a cluster of small residences with workshops including Samuel Moulton, Master Rigger (listed at number 4 Hale Street); Robert Stanley, Warehouseman (number 6); William

Astley, Copper Plate printer (number 8); Hugh Jones, Tailor (number 10) and George L. McDowie, a labourer living at number 12. These structures fall within the present day footprint of Eagle Star House, which is now sited on 57/59 Dale Street.

- 3.3.12 Adjacent to this small group of residences is Avenue Chambers, which would today fall within the perimeter of the footprint for Eagle Star House at 57/59 Dale Street. This building complex appears to have been of a similar design to Graecian Chambers and also functioned as a purpose-built office block. Listed at Avenue Chambers in 1870 were: Edward Roland and Co, tea merchants, Frederick Fothergill, tea dealer; R Bellwood and Sons, estate agent; William Hunter, cotton broker; JB Pride, law stationer and Robert Rowlands, an iron merchant (Gore 1870, 727). Listed at numbers 14 – 24 Hale Street respectively are James Visnee, printer and bookbinder; Patrick Mooney, warehouseman; James Larkin, engineer and John Shackleton, the Master Bailiff.
- 3.3.13 In 1874 the Hale Street Warehouse was constructed in a small court environment, accessible from Dale Street via both Vernon Street at the eastern limit of the site and Bachelor Street, located to the west of the site and running parallel with Vernon Street. At this time, according to the listings in the Gore's Directory of 1874, the streets around the site, including Hale Street and Vernon Street were still populated by small businesses, engaged in production and trade, although Graecian Chambers was now listed as Graecian Buildings.
- 3.3.14 By 1888, the whole site was still occupied by small-scale trade and industrial establishments, with Graecian Chambers (at this point the name has returned to that of Chambers rather than Buildings) and Avenue Chambers still providing office space much like Vernon Chambers located over The Saddle Hotel at 69 Dale Street (now the Vernon Arms Public House). Both of which were isolated from the rest of the block by the presence of a narrow alley which is marked as Nixon's Place on the 1888 Goad Map (de Figueiredo 2010, 4). Numbers 61/63 Dale Street are listed as being occupied by a printers and stationers office and number 65-67 Dale Street was a combination of shop premises and a hotel partly converted from courtyard dwellings at the rear (*ibid*). Much like the 1888 Goad Map (included in the de Figueiredo report), the 1890 OS Map (Fig 9) shows the boundaries of each structure on the site, comprising 19 structures in all, including small dwellings, a public house, warehousing, a hotel and two bespoke office blocks (Avenue and Graecian Chambers).

3.4 TWENTIETH CENTURY

- 3.4.1 In 1900, Gore's Directory provides a comprehensive list of occupants for the site, although at this time, the buildings occupying the plot are still different to the ones present today. Number 57A Dale Street, in the building formerly known as Graecian Chambers, occupies the area of site which runs from the Dale Street frontage and along the side of the pedestrian alleyway that represents a continuation of Hale Street although it is occupied by the warehouse element of the west-facing elevation of 57/59 Dale Street, Eagle Star House. In it were a variety of professional gentlemen whose professions included artist, architect and civil engineer.

- 3.4.2 Number 57 Dale Street would have been a small shop frontage, but Gore does not assign a specific name to the building; it was occupied by Robert Skinner whose profession was listed as a tailor. The adjacent building was split into 59 and 59A Dale Street, with number 59 occupied by Joseph Duffy, an oyster dealer, and 59A listed as the premises of William H Carr, a jeweller and watch maker. Number 61 was occupied by WH Smith and Son, bookseller and newsagents and William H Lloyd, letterpress and lithographer, suggesting a continuity of function from that noted in 1888. Number's 63 and 69 are occupied by two victuallers listed as James C Spring and Henry Jones respectively. There is no listing for numbers 65 and 67.
- 3.4.3 In 1906, the Pioneer Building was constructed by Liverpool-based architects Woolfall and Eccles, and was commissioned by the Pioneer Assurance Company. The company operated from 1891-1974 and occupied the building during this period, although they were not the sole occupants of the premises (de Figueiredo 2010, 7). This was followed by the demolition of the properties at the west side of the block, including 57/59 Dale Street, Graecian Chambers, Avenue Chambers and the small dwelling houses between. The plot between Dale Street and Hale Street was cleared and in the 1930s (Plate 1), Eagle Star House was constructed (the precise date for construction is unknown), with a footprint which extended across the full width, and almost the full length, of the plot.
- 3.4.4 The modern arrangement of buildings fronting onto Dale Street was completed with the addition of 61/63 Dale Street, probably in the late 1930s or 1940s (*ibid*). This building is unnamed and is the least charismatic of the group with a flat, vertical face typifying the efficient, clean lines favoured by post-Second World War architecture.
- 3.4.5 In 1966 the internal structure and function of the Pioneer Building was modified with the conversion of the ground floor and basement to offices and general alterations to the ground floor shop. In 1975 the main ground floor space was altered to accommodate a printers' work shop and ancillary offices.
- 3.4.6 The Pioneer Building was purchased in 1980, on speculation, by the Royal London Mutual Assurance Society Ltd, who, in 1982, modified and repaired the Dale Street facade of the structure. This involved the cleaning of the brick and stone facade as well as the repairing, reconstruction and redecoration of the shop front and entrances and the decoration of all window frames and downpipes; simultaneously the framing above the shop front was reconstructed to form two recessed panels. In 1985 the building was modified and refurbished with the addition of numerous partition wall on all floors to create retail and office space. In 1991 the ground floor frontage was once again replaced and a fire escape stair was added to the rear of the building. The former site of number 7 Vernon Street was demolished at this time and the cleared area was enclosed to provide storage space to the rear of the building. The remnants of the north-facing wall of 7 Vernon Street is still present today and forms the boundary between the site and the Hale Street Warehouse car park.
- 3.4.7 Number 61/63 Dale Street underwent a similar spate of remodelling and refurbishment as the demand for business premises changed, and in 1983 the front and rear elevations of the showroom premises were modified with a new Dale Street shop frontage being added. A new boundary wall was also constructed to the rear of the premises, enclosing the open land adjacent to the Hale Street Warehouse. In 1985 this area was laid out as a car park and the east-and west-

facing gable walls of the Hale Street Warehouse were rendered, with the addition of a new outrider at the rear of the structure providing toilet facilities and a new stepped entrance to the north side of the property. At the same time, the character of the area was further altered with the construction of Number 2 Moorfields (still in existence to the west of the site), which effectively obliterated the original footprint of 6 Moorfields, 2, 4 and 3-11 Bachelor Street (Bachelor Street disappeared altogether at this stage) and 45-55 Dale Street. The existing Moorfields building which houses offices and provides access to Moorfields train station is located on the west side of the site, forming the modern western boundary of Hale Street. Further modifications were made to the east-facing elevation of the Hale Street Warehouse in 1986 with the addition of a lean-to car port structure, which has subsequently since been removed.

- 3.4.8 Eagle Star House was modified relatively recently with the total refit of the first-fifth floors in 1997 and the addition of two new entrances on the ground floor facade in 1998, when the building was split into a SPAR convenience store and office premises. More modern alterations to the existing structures, now occupying the block, are recorded in some detail in the City Council Planning Archive and it is from there that most of the information concerning the later alterations was obtained.

4. WATCHING BRIEF RESULTS

4.1 INTRODUCTION

- 4.1.1 Following on from the demolition phase, the objective of the watching brief was to identify, investigate and record any archaeological remains encountered during the groundworks for the proposed development. All of the standing buildings on the site have been recorded and discussed in-depth in the building survey report (OA North 2011) and so there is only a limited reference to these structures which were under demolition when the watching brief was being carried out. The following, therefore, is a summary of the findings of the below ground watching brief which examined the open area at the northern limit of the site (Fig 10). A list of contexts used has been provided in *Appendix 2*.
- 4.1.2 The watching brief was undertaken in conjunction with the complete demolition of the Hale Street warehouse and 59/61 Dale Street, as well as the partial demolition of Eagle Star House (55/57 Dale Street) and the Pioneer Building (63/65 Dale Street). This presented a number of logistical challenges on site regarding the availability of space and the positioning of the exploration areas. Consequently, although the part of the site that was subject to archaeological investigation was very small, it was excavated in several stages as a series of small overlapping areas, rather than a single an open area excavation. The site was investigated as five small areas of varying sizes (Fig 10), the two largest areas being Trench 1 and 2 at the north-eastern limit of the site, each of which measured an average area of approximately 50m².

4.2 RESULTS

- 4.2.1 The first stage of the excavation process involved stripping the modern tarmac and concrete **001**, across the majority of the site. This process quickly revealed *in situ* granite cobble setts, **002**, and associated large rectangular granite kerbs which marked the former southern boundary of Hale Street or possibly the continuation of Hale Street into the yard outside the Hale Street Warehouse entrance (Figs 11 and 12; Plate 3). The surface was largely intact but had been truncated along the northern and southern limits by modern service trenches and drainage, some of which had been installed relatively recently to facilitate the on-site welfare accommodation.
- 4.2.2 **Area 1:** located at the northern limit of the site, in front of the site entrance (Fig 10 - 11), and orientated north-east/south-west on its long axis, this area measured approximately 6.9m by 7.5m. The earliest deposits observed within the trench comprised a series of made ground layers upon which several brick warehouse foundations had been constructed. The made ground deposits were largely sandy clay deposits overlain by a layer of demolition backfill (largely comprising bricks, mortar and silty clay) **010**, within the perimeter of the brick foundations. The brick foundations **005**, **007**, **008** and **009** together comprise the remains of two separate structures, probably small warehouses, with wall **007** representing the eastern gable of one structure and walls **008** and **009** having been added against its exterior face at a later date to create some sort of small room or outrigger. Brick wall **005** was

located at the northern limit of the area and was heavily truncated by modern services, as well as sustaining damage during the machining process to remove the cobbled roadway **002**.

- 4.2.3 The foundations were constructed of handmade red-brick, bonded and crudely pointed using buff coloured sandy lime mortar (Plate 4); all the walls had been constructed in an English Garden Wall style bond. The western wall **008**, was orientated north-west/south-east and was the most substantial of the three walls. It was abutted by, rather than bonded to, wall **007**, suggesting that it was originally a gable wall for an pre-existing building; historic photographs of the area (Plate 1) indicate a somewhat organic evolution of sheds and outhouses within this area. This structure was three courses wide (0.35m), in excess of eight courses deep (0.68m) although the base of the wall was not identified during the watching brief because it extended beyond the formation level, and was in excess of twelve courses long (3.4m). Wall **007** (orientated north-east/south-west) and truncated wall **009** (orientated north-west/south-east and located parallel to wall **008**) were both less substantial structures, being only two courses wide and of a poorer quality construction, suggesting that they were added to the exterior of a more substantial building. Walls **007** and **009** were bonded indicating that their construction was contemporary. Located to the north-west of **008**, located just beneath cobbled surface **002** and associated clinker bedding layer **003**, was the remnants of a flagged stone yard or pathway **006**.
- 4.2.4 **Area 2:** was located to the north-east of Area 1 (Figs 10-11) where the main archaeological deposits were also sealed beneath the granite cobble setts **002** which form part of Hale Street (also identified extending across Areas 1, 4 and 5 on an east/west orientation). The earliest deposit encountered within this area was a sandy pale orange-brown clay natural **012**, which was at a depth of 1.2m below the present ground surface at the north-east limit of the site where Hale Street and Vernon Street intersect.
- 4.2.5 Cutting the natural **012**, on a roughly north/south orientation, was an L-shaped feature **013**, which has been tentatively identified as a possible field boundary or drainage ditch (Fig 11). The ditch spanned the full length of the excavated area and was seen to curve to the north-east at the northern limit of Area 2. It is unlikely that this feature survives to the south due to the fact that the whole southern half of the block was cellared during the 1860s. This feature appears to have been truncated by later levelling and construction activity and only measured 0.15m deep. The cut of the ditch **013**, had been backfilled with a very moist blackish-grey plastic clay with a high organic content. This deposit was also found to contain fragments of wood, and heavily abraded red brick, animal bone and pottery. A small section of the deposit was excavated for pottery in order to provide dating evidence. The truncated ditch and natural clay were both overlain by a later of mixed homogeneous clay **011**, measuring 0.35m thick which may represent a layer of redeposited natural. This in turn was overlain by a heterogeneous deposit of sand rubble over burden **004** which is a continuation of the deposit identified within Area 1.
- 4.2.6 **Area 3:** was located to the south of Area 1 and 2 and was largely characterised by layers of rubble and made ground deposits, much of which had been generated recently during the demolition phase. This area was excavated as a discrete 2.5m x

2.5m trench after the chance discovery of a substantial void which was quickly identified as a capped well shaft.

- 4.2.7 The earliest structure identified within this area was a red brick well shaft, **016**, and associated red brick plinth **018** (Fig 12). The well was 25m deep, and 0.8m in diameter; exploration from the surface showed that the well was only brick-lined for the upper 9m, with the lower 16m of the shaft cutting through the yellow sandstone bedrock with the bottom few metres opening into a wider bell-shaped aquifer. The well was dry when encountered and was totally devoid of infill or dumped cultural material. The well shaft was surrounded by a red brick plinth constructed using handmade red bricks with a sandy lime mortar bond; the bricks were placed in a haphazard irregular bond. The well is not indicated on any historic maps for the area but it is likely that it would originally have been placed within an open courtyard to provide a fresh water supply for the block of buildings situated between Dale Street, Hale Street and Vernon Street.
- 4.2.8 The well was originally identified capped with a domed brick structure **015** (Plate 6), which was also constructed of handmade red brick and measured 1.32m in diameter, standing to a height of 0.6m. Removal of the capping provided a clearer view of the brick lined well shaft and the surrounding plinth **018**. The well was constructed from carefully shaped red bricks, many of which had been formed to creating the internal curve of the well shaft. The bricks had been bonded with a patchy mortar, and given the sporadic application of the mortar this might suggest a repair rather than a part of the original construction process (Plate 8). The diameter of the well was approximately 0.8m.
- 4.2.9 The well was bounded to the south by a number of surfaces constructed in a mixture of materials and extending for a distance of approximately 1m. To the south of the well was an east/west orientated yellow sandstone kerb or edging stones, **021**, a row of hand made red brick headers, **022**, running parallel to **021** and an east/west orientated row of granite tiles **023**, also parallel to **021** and **022**. The juxtaposition of these mixed surfaces all seem to suggest a boundary with the granite tiles perhaps more suggestive of interior flooring than an external surface such as a yard or alleyway. All of these surface respect the well plinth **018**, suggesting that the well was still visible and in use at the time they were laid down. The mixture of materials used to create this area might suggest a threshold or some sort of division between an interior (domestic or warehouse) and exterior (yard) space. The materials and their placement, along with the quality of the bedding might also suggest that this area of surface was constructed from a mix of recycled materials, not originally intended for this purpose.
- 4.2.10 To the west of well **016** was a yellow sandstone foundation wall **019** on top of which was constructed a later hand-made red brick wall (Plate 7). Yellow sandstone wall **019** was orientated north/south and was constructed of large roughly-hewn (or possibly rusticated) blocks of yellow sandstone three courses long (1.82m) and three courses deep (0.6m). Only the west-facing elevation of the wall was exposed with the east-facing elevation being obscured by the construction of the well shaft directly against it. Directly on top of this wall was a handmade red brick wall **020**, which was seven courses long, two courses wide and eight courses high and was constructed in an irregular style with a buff coloured sandy-lime mortar bond. There was evidence of a returning wall stub at the south-western limit of the structure. No surfaces or floor levels relating to these two walls were

encountered during the excavation. This small group of archaeological features was sealed with a heterogeneous overburden deposit of mixed rubble and mid-brown sandy clay, **004**.

- 4.2.11 **Area 4:** was located at the north-west corner of site in the area between the site of the former Hale Street Warehouse and the extant electricity substation. The earliest deposit identified within this area was a disturbed sandy orange-brown clay natural **012**, overlain by a deposit of mixed clay embedded with fragments of crushed brick and small stones **025**. As with the other areas, once the tarmac **001** had been stripped, the first deposit encountered was the remainder of the cobble setts **002** which represent Hale Street on its earlier alignment. Although there was no evidence of cellars or brick structures within this area, Area 4 was located close to the remains of the cellar for the Hale Street warehouse (OA North 2011) and it is likely that the ground in this area would have been disturbed and the ground level raised during the course of construction work for the warehouse built in 1874 and the later outrigger constructed in the 1950s.
- 4.2.12 **Area 5:** was located to the west of Area 2 and to the east of Area 4, parallel to modern Hale Street and it measured approximately 5m by 8m. Excavations within this area immediately encountered the continuation of the cobble sett surface **002** and this surface associated with the original orientation of Hale Street sealed all earlier deposits. No structures or features of note were identified within this area (Plate 9).
- 4.2.13 The area at the western limit of the site was not subjected to an archaeological investigation as it is currently occupied by the site welfare facilities and offices.

4.3 FINDS

- 4.3.1 A number of finds were retrieved during the excavation process including thirty sherds of pottery and a four pieces of kaolin clay tobacco pipe stem.
- 4.3.2 Within Area 1 (Fig 10 - 11), 12 sherds of pottery were retrieved from deposit **004**, including four sherds of slip-coated ware, two sherds of mottled ware, two of slipware, one sherd of blackware and three sherds of refined white earthenware. The refined white earthenware dates to the mid nineteenth century, however the other sherds of pottery within this context have an early-mid eighteenth century date (c. 1750). Also within Area 1, six sherds of blackware were retrieved from **010**, the infill of the cellar/foundation wall footprint **007-009**. These sherds are not closely dateable as this type of pottery was widely produced in great quantity between 1650 and 1850 within the Merseyside area. Two fragments of undated kaolin clay tobacco pipe stem were also retrieved from this deposits.
- 4.3.3 Within Area 2 (Fig 11), two large body sherds were recovered from the infill **014**, within truncated ditch **013**. The first sherd was a slip coated glazed red earthenware and the other was a sherd of black glazed buff ware, both of which can be dated to the eighteenth century. Two larger fragments of kaolin tobacco pipe stem were also retrieved from **014**; however, neither stem has any distinguishing makers mark so it is not possible to closely date these artefacts.

5. CONCLUSION

5.1 DISCUSSION

- 5.1.1 All of the archaeological structures and deposits identified during the course of the watching brief were sealed beneath the cobble stone setts of the pre-Second World War alignment of Hale Street. This road was contemporary with the remodelling and realignment of Dale Street during the 1860s indicating that all of the deposits below it predate the major Victorian remodelling of the business district.
- 5.1.2 It is likely that the exterior surfaces and brick foundations represent the remains of smaller warehouses and ancillary structures which were demolished in order to facilitate the expansion of Dale Street as part of the Liverpool Improvement Act of 1867. This was done in order to create the broader Victorian thoroughfare which still remains today with the majority of the buildings to the east of the Town Hall being constructed after 1867, although moderate improvement works were undertaken after 1860. The original narrow aspect of Dale Street is now only evident from the location of the Town Hall, constructed in 1754, the frontage of which projects noticeably into Dale Street, rather than respecting the later, broader street plan.
- 5.1.3 The earliest deposit identified during the course of the excavation was the sterile natural clay, **012**, which was observed in Area 2 and Area 4. The earliest archaeological deposit identified was a truncated curvilinear ditch **013**, also within Area 2. This feature cut the natural clay, **012**, and was identified at a depth of 1.2m below the present ground surface, curving from the north-east to the south-west within the perimeter of the trench. The ditch was found to contain a dark organic material **014**, and contained several sherds of pottery dating from the mid-seventeenth to mid-eighteenth century (*Section 4.3.3*).
- 5.1.4 Given the limited nature of the excavations and the relatively small area of the excavation it was not possible to determine the full extent or function of the ditch. It may represent a simple drainage ditch within a plot or garden such as those present on the Okill map of 1650 (Fig 3) and Eye's Map of 1765 (Fig 4) or it may represent the last vestiges of a land boundary from the 1780s onwards when land was being subdivided and parcelled off according to the requirements of the burgeoning merchant classes in the area. Ditch **013** and organic fill deposit **014** represent all that remains of the early post-medieval landscape within this area of Dale Street and also within the development footprint as the rest of the area had been truncated by extensive cellar construction undertaken after 1860.
- 5.1.5 The most complete structure identified during the course of the excavation was a handmade brick-lined well shaft, with surrounding plinth and an intact domed brick capping. The well was 25m deep and dry, but and empty of backfill or detritus. Post-medieval wells have been commonly found from interventions elsewhere in the city, specifically at Chavasse Park as part of the Grosvenor's Paradise Project redevelopment (OA North 2010), Mann Island (OA North 2011) and Hope Street (OA North 2012) where in excess of 20 wells have been excavated to date. This is by far the deepest and best preserved structure of this type yet identified within the city centre. Wells identified elsewhere within the city typically had a maximum

depth of between 8-16m depending on their location and their proximity to the water table.

- 5.1.6 Historic images, such as the view of Hale Street and Vernon Street from Cheapside (Plate 1), demonstrate the organic and cramped nature in which sheds, lean-to buildings and warehouses had developed within the study area along the northern side of Dale Street. It is likely that much of this earlier activity has been severely truncated and now only survives in ever decreasing pockets of ground between larger buildings and beneath earlier roadways and footpaths.

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APPENDIX 1: PROJECT DESIGN

1. INTRODUCTION

- 1.1.1 **Background:** a proposed development of 57-67 Dale Street, Liverpool, for the construction of a hotel and a multi storey car park behind, will entail the demolition of much of the fabric of 57-67 Dale Street, although the first whole bays, and elevations, of Eagle Star House (57-59 Dale Street) and Pioneer Building (67 Dale Street) will be retained. None of the buildings are listed, but the site is within the Castle Street Conservation Area and is within the Liverpool Maritime Mercantile World Heritage Site. An assessment of significance of the structures, in the form of a PPS5 statement, has been compiled by Peter de Figueiredo which has served to inform the design of the new build.
- 1.1.2 An outline documentary study has been compiled for the site as part of the PPS5 statement (de Figueiredo 2010) which assessed the archaeological and historical significance of the site. It is evident that the buildings have a broad range of fabric reflecting the development of the site from the late nineteenth century; however, it is evident that there are considerable amounts of recent fabric, and that the historic fabric is extensively obscured by later surfaces, making it difficult to analyse the development of the structures.
- 1.1.3 **Building Survey:** the planning condition (application no 09F/2531) stipulates: *No site works, development or demolitions shall be undertaken until the implementation of an appropriate programme of building recording and analysis has been agreed in writing with the local planning authority, to be carried out by a specialist acceptable to the local planning authority and in accordance with an agreed written brief and specification. The documentary record to be submitted to the local planning authority.*
- 1.1.4 Discussions with the planning authority have indicated a requirement for an English Heritage level 3 (2006) survey of those elements of the structure that will be affected by the development. This will require the production of plans, cross sections and elevations, as well as an annotated photographic record, and an illustrated final report. The present project design outlines the requirements for a programme of building recording to satisfy the planning condition.
- 1.1.5 **Watching Brief:** the planning condition (application no 09F/2531) stipulates: *the developer shall, until the completion of the development afford access at all reasonable times to any archaeologist nominated by the local planning authority and shall allow the observation of the excavations and the recording of items of interest and finds.*
- 1.1.6 Following demolition there is a requirement to maintain an archaeological watching brief during any intrusive ground works, and to record any archaeological remains revealed. The present project design outlines the requirements for a watching brief during this phase of the work.

1.2 HISTORICAL BACKGROUND

- 1.2.1 The site is currently occupied by five buildings of which four will be directly impacted by the development. These buildings include:
- 57 – 59 Dale Street (c 1920's) now Eagle Star House
 - 61 – 63 Dale Street (c 1950's)
 - 67 Dale Street, The Pioneer Buildings constructed c1860?
 - Hale Street (no number given), two storey red brick house with moulded brick panels and sandstone lintels constructed 1874.
 - Vernon Arms Public House (formerly known as Vernon Chambers). The public house will be retained and is not included in the development plan although its context will be affected by the development.

- 1.2.2 Dale Street was one of the earliest streets (probably dating from the 13th century) constructed in Liverpool and formed part of the medieval “H-shaped” street plan, including Water Street, Chapel Street, Tithebarn Street, Castle Street, Jongler Street and Old Hall Street. These eight streets formed the core of the Medieval town prior to the construction of the Old Dock. With the construction of the Old Dock in 1715 it was only a matter of time before Liverpool’s commercial district underwent a dramatic expansion with this area (close to the Town Hall and Exchange) providing the ideal site for Banks, Insurance and Shipping Line offices as well as warehouses and offices for the new Mercantile elite (De Figueiredo 4, 2010). Dale Street was close to the original custom house and also allowed quick access to the north shore and the newly developed docks via Water Street or via Castle Street to South John Street.
- 1.2.3 In 1786 the Corporation obtained its first Improvement Act which gave it powers to widen older streets. At this time the west side of Castle Street was rebuilt along its present line and at the same time parts of Dale Street and Water Street were also widened (Sharples 11, 2004). A further act of 1826 made important civic improvements possible including the redesign of Derby Square, Lord Street, South and North John Street and parts of Dale Street (Sharples 15, 2004). The alterations made in 1826 paved the way for many of the elements of the Dale Street still visible today as a series of increasingly grand Victorian Structures were built within the business district.
- 1.2.4 **57 – 59 Dale Street:** originally 57 Dale Street was occupied by a building known as Grecian Chambers which appears to have served as a mixed use office block for a number of small businesses. These include in 1871, R. Holden and Cleaver a firm of solicitors (Fulton 58, 1871). *The Kalendar of the Royal Institute of British Architects* (1984) notes that in 1875 Grecian Chambers was occupied by the architect William Parslow. Parslow shared his premises with J.D. Worship, a solicitor (Green 219, 1875). This plot is now occupied by the Eagle Star House and is a six storey building including a large basement and additional utilities rooms on a flat roof. The south facing elevation faces onto Dale Street and is an undecorated elevation clad in Portland Stone and probably dates to the 1930’s (De Figueiredo 8, 2010). The largest elevation of this building is the west facing elevation which overlooks the pedestrian alley leading to Hale Street. This elevation also reflects the functional nature of this structure with a combination of white glazed brick and traditional brown brick.
- 1.2.5 **61-63 Dale Street:** this building is the most modern of all the structures included within the development and probably dates to the 1940’s. The building is a five storey structure including a large basement. It is constructed with a steel frame and concrete floors and is clad in Portland Stone and granite. The elevation is strongly horizontal and is functional in design
- 1.2.6 **65-67 Pioneer Buildings:** the Pioneer Buildings were built in 1906 and were designed by Liverpool architects Woolfall and Eccles for the Pioneer Assurance Company. John Woolfall and his partner, Thomas Edgar Eccles, had their practise at 60 Castle Street and so were well placed to undertake the design of the building (Brodie 103, 2001). The building is five storeys with a steel frame and concrete floors and is clad in sandstone and red brick. The front elevation is designed in a free Arts and Crafts style, mixing both English Baroque and Tudor motifs (De Figueiredo 10, 2010). The main features of the building’s south facing elevation are narrow mullioned and transomed windows grouped in two pairs on either side of a triple window in the centre beneath a central broken pediment which crowns the fifth floor. The ground floor frontage has been altered and filled with modern large modern windows (De Figueiredo 10, 2010). The secondary frontage of the structure is east facing on the west side of Vernon Street. This frontage is notably less grand and is a simple red brick elevation interspersed with regular sash window bays. This frontage encompasses the east facing elevation of the ground floor of the Vernon Arms Public House.

2. OBJECTIVES

- 2.1 **Fabric Survey:** the principal objective of the archaeological survey is to provide an understanding of the historical development of the site, and to identify significant architectural elements within the surviving fabric. The present survey does not include the Vernon Arms Public House, which will not be directly affected by the development.
- 2.2 The project aims are based on the English Heritage guidelines (2006) for undertaking Level 3 surveys and will have the following requirements:

- demonstrate the buildings' plan, form, fabric, function, age and developmental sequence from the historical and archaeological evidence available;
- identify key architectural features within the surviving fabric;
- the production of appropriate drawings, which would include plans highlighting historic fabric for all floors - the basement, the ground, first and second floors. Also there would need to be a general site plan of the buildings.
- the production of a cross section through the north/south axis of the building.
- the production of elevation drawings of the principal facades.
- undertake a comprehensive and detailed photographic survey of the surviving fabric, which would include general views, external oblique views, photographs of the principal rooms, external and internal detail pertinent to the buildings design.
- the production of a final report and project archive, which would include an account of the buildings overall form, its past and present usage and its development, and a full bibliography

2.3 **Watching Brief:** the following programme has been designed to provide a suitable level of archaeological observation, recording, and response during ground works linked to the proposed development. In the event that a monument of importance is discovered then an agreement will need to be agreed between the Merseyside Archaeologist (or planning authority) and the client as to the level and means of preservation. The required stages to achieve the recording works are as follows:

- to record accurately any surviving archaeological features or deposits by means of detailed observation and recording. To record the presence of buried features by appropriate recovery techniques, where applicable.
- in the event of discovering significant features a programme of rapid excavation will record the features within a wider context to allow an appropriate understanding of the remains.
- a full written report will assess the significance of the data generated by the entire programme of work, in a local and regional context, and will be suitable for deposition as a permanent archive of the work undertaken.

3. METHODOLOGY

3.1 Historical Background

3.1.1 An historical background is an essential prerequisite of the proposed survey, and although a basic documentary study has been undertaken by Peter De Figueiredo, this does not provide sufficient detail to inform the development of the buildings as required by the Level 3 survey requirements defined by English Heritage (2006). It is therefore proposed to undertake a desk-top assessment to examine primary and secondary sources such as maps, photographs and descriptive records. The documentary study will inform both the building survey and the watching brief and will examine the development of the site prior to the construction of the present buildings and also the history of the present buildings through to the present.

3.1.2 **Documentary Material:** this work will include consultation of the Liverpool Records Office if possible; however, the documents of the record office have been deposited into store while the Liverpool City Library is refurbished and it is not known what access will be afforded. Subject to availability the records to be consulted will include;

- relevant published sources. To include articles, and regional and local journals
- relevant unpublished documentary sources. To include, where appropriate, reports compiled by heritage conservation professionals and student theses
- data held in local and national archaeological databases, specifically the Merseyside HER.
- cartographic, photographic and any pertinent drawings

- consultation with the land owner, to draw upon any records within his possession
- contact will be made with the Eagle Star historian to identify any records relating to 57-59 Dale Street.

3.2 Fabric Survey

- 3.2.1 A detailed commercial survey has been undertaken of the building by Survey Systems Ltd, and on examination this would appear to be adequate, to serve as the basis for archaeological enhancement, to satisfy the English Heritage Level 3 requirements. This has provided plans at all levels, and elevation drawings of the façades but it has not produced any cross sections; it is anticipated, therefore, that a single cross section through the building will need to be compiled by instrument survey. Beyond that single drawing, any requirement for the acquisition of supplementary survey detail will be undertaken by the manual enhancement survey of existing drawings.
- 3.2.2 Manual survey is typically applied to furnish additional architectural detail to existing drawings and is appropriate for the editing of plans and cross sections. Paper plots of existing drawings will be produced from the digital copy for enhancement in the field. Detail from the completed field drawings will be digitised to allow the correction of the digital copy.
- 3.2.3 The drawings will be manipulated in AutoCAD software. The advantage of a CAD system is that it allows for efficient manipulation and editing of drawings. The adoption of a layering system has significant benefits during the analysis stage as it allows for the display of information such as feature types, fabric and phasing as necessary to the requirements of the analysis, without the necessity to produce further drawings. Finished drawings can be plotted at the required scale or sheet sizes. The survey will result in the production of the following drawings:
- Site Plan
 - Basement Plan
 - Ground Plan
 - First Floor Plan
 - Second Floor Plan
 - Third Floor Plan
 - Fourth Floor Plan
 - Fifth / Loft Space Plan
 - East/West Cross Section
 - North/south façade elevation
 - East/west façade elevation
- 3.2.4 New survey of the east/west cross section will be by means of a reflectorless total station, which is capable of measuring distances to a point of detail by reflection from the wall surface, and does not need a prism. The instrument to be used will be a Leica TCR805 reflectorless total station, which emits a visible laser beam that can be visually guided around points of detail. The digital survey data will be captured within a pen computer running TheoLT software, which allows the survey to be directly inserted into AutoCAD software for the production of final drawings. Additional survey will be carried out if necessary using a hand held Leica Disto laser measure.
- 3.2.5 **Photography:** in conjunction with the archaeological survey a photographic archive will be generated, which will record significant features as well as general views. This photographic archive will be maintained using high-quality digital cameras with 10 mega pixel resolution. The use of a digital camera provides very effective manipulation of photographic images, and these will be used in the report. The use of photography in this way considerably enhances the usability of a database and greatly assists the analysis of the monument. In addition photographs will be taken on black and white film for archival purposes. The photography will provide the following views:
- (i) the general external appearance and wider setting of the buildings;

- (ii) oblique views of all the external elevations, perpendicular views only if necessary and appropriate;
- (iii) the overall appearance of the principal rooms and circulation areas, right angle views of internal elevations only if necessary and appropriate;
- (iv) any external or internal scaled detail, structural or architectural, which is relevant to the design, development and use of the buildings, and which does not show adequately on general photographs;
- (v) any internal scaled detailed views of features of especial architectural interest, ephemera, fixtures and fittings, or fabric detail relevant to phasing the buildings which does not show adequately on general photographs.

3.2.6 **Description:** a detailed description of the buildings will be carried out to English Heritage Level 3 guidelines, utilising *pro-forma* sheets. This provides for a comprehensive analytical account for buildings of special importance and the following methodology will be followed.

3.2.7 The written account will provide the understanding required in order to place the building in its historical, architectural and cultural context. The descriptive record will include the following accounts:

- A general description of the buildings, which will include details of the plan, form and function. Allied to this, a detailed description of the materials used and development sequence and phasing, including any alterations, repair and rebuilding;
- An account of the wider context within which the buildings are situated. For example, its relationship to places and buildings within the local area, as well as its historical relationship to the area;
- An appropriate description of each individual room/discrete space and component.

3.3 WATCHING BRIEF

3.3.1 **Methodology:** a programme of field observation will be maintained during all ground works to identify and accurately record the location, extent, and character of any surviving archaeological features within the ground works for the proposed development. This work will comprise the observation of the process of excavation for these works, the systematic examination of any subsoil horizons exposed during the course of works, and the accurate recording of all archaeological features and horizons, and any artefacts, identified during observation.

3.3.2 During this phase of work, recording will comprise a full description and preliminary classification of features or materials revealed, and their accurate location (either on plan and/or section, and as grid coordinates where appropriate). All archaeological information collected in the course of fieldwork will be recorded in standardised form, and will include accurate national grid references. Features will be planned accurately at appropriate scales. A photographic record will be undertaken simultaneously. The recording techniques and procedures employed by OA North for such detailed recording represent current best practice. All features and structures will be accurately planned using a survey grade differential GPS (Leica 1200) which is accurate to +/- 0.02m.

3.3.3 In the event that a significant find or structure is identified it will be fully recorded beyond the anticipated maximum depth of the development ground works if necessary and also beyond the extent of the footprint of the development in order to provide an appropriate context for the remains. This may necessitate stopping works in the area of the find to enable the implementation of this recording. It may prove necessary to call in additional archaeological support if a find of particular importance is identified. This would only be called into effect in agreement with the Client and the Merseyside Archaeologist (or planning authority) and will require a variation to costing. In normal circumstances, field recording will also include a continual process of analysis, evaluation, and interpretation of the data, in order to establish the necessity for any further more detailed recording that may prove essential.

3.3.4 **Finds Processing:** finds recovery and sampling programmes will be in accordance with best practice (current IfA guidelines for finds work). All typologically significant and closely datable finds will be contextually recorded. All artefacts and ecofacts will be handled and stored according to standard practice (following current Institute for Archaeologists' guidelines) in order to minimise

deterioration. Finds storage during fieldwork and any post-excavation assessment and analysis (if appropriate) will follow professional guidelines (UKIC). All finds will be washed, marked and packaged as appropriate. Small finds will be individually packaged, in a manner appropriate to the find type.

- 3.3.5 The artefact assemblage will be examined by OA North finds specialists, and the potential for further examination will be assessed. A summary report on the significance, character and date range of the assemblage will be generated.

3.4 REPORT PRODUCTION

- 3.4.1 **Final Report:** a written synthetic report will be submitted within five weeks of completion of the final stage of works. The final report will present a well-ordered synthesis of the programme of investigation, including both the results of the building survey and the watching brief. The report will include a full index of archaeological features identified in the course of the project, with an assessment of the overall stratigraphy, together with appropriate illustrations, including detailed plans and sections indicating the locations of archaeological features. Any finds recovered from the excavations will be assessed with reference to other local material and any particular or unusual features of the assemblage will be highlighted.

- 3.4.2 The report will also include a complete bibliography of sources from which data has been derived. This report will identify areas of defined archaeology. An assessment and statement of the actual and potential archaeological significance of the site within the broader context of regional and national archaeological priorities will be made. The report will include the following:

- ◆ a site location plan related to the national grid;
- ◆ the dates on which the fieldwork was undertaken and by whom;
- ◆ a concise, non-technical summary of the results;
- ◆ table of contents;
- ◆ acknowledgements;
- ◆ the precise location, address and NGR of the site;
- ◆ project background and historical context;
- ◆ a description of the methodologies employed, work undertaken and results obtained;
- ◆ an appropriate description of the results of the investigation, including the physical characteristics and condition of each site component;
- ◆ an appraisal of the quality and reliability of the data;
- ◆ an overall interpretation of the generated data and preliminary conclusions reached;
- ◆ recommendations for further work;
- ◆ plans, elevations, section drawings and photographs at an appropriate scale;
- ◆ the report will also include a complete bibliography of sources from which data has been derived;
- ◆ appendix/gazetteer of raw data for each site component generated during the investigation, illustrated as appropriate;
- ◆ a copy of the project brief will be included in the appendices;
- ◆ a copy of this project design in the appendices, and indications of any agreed departure from that design;
- ◆ a summary of the project archive;
- ◆ copies of any appropriate photographs and drawings in the archive.

- 3.4.3 Prior to the dissemination of the final report, plans, elevations and/or data will be made available to the client during the course of the works. Four bound copies will be submitted (including two for

the client and one for the Merseyside Historic Environment Record and two copies of the final version in pdf format will be submitted on CD. CAD files in AutoCAD.DWG format will be included on the discs.

3.5 Archive

- 3.5.1 The results of all archaeological work carried out during fieldwork will form the basis for a full archive to professional standards, in accordance with current English Heritage guidelines (Management of Research Projects in the Historic Environment (MoRPHE) 2006). The project archive represents the collation and indexing of all the data and material gathered during the course of the project. It will include summary processing and analysis of all features, finds, or palaeoenvironmental data recovered during fieldwork to the appropriate level. OA North conforms to best practice in the preparation of project archives for long-term storage. This archive will be provided in the English Heritage Centre for Archaeology format and a synthesis will be submitted to the Merseyside Historic Environment Record (the index to the archive and a copy of the report). OA North practice is to deposit appropriate elements of the original record archive of projects (paper, magnetic and plastic media) with the appropriate County Record Office, and a full copy of the record archive (microform or microfiche) together with the material archive (artefacts, ecofacts, and samples) with National Museums Liverpool (NML). The actual details of the arrangements for the deposition/loan and long term storage of this material will be agreed with the landowner and NML. The archive will be compiled in accordance with the National Museums Liverpool (NML) 'Guidelines for the Transfer of Archaeological Archives to National Museums Liverpool V3' (revised 2010) and followed as part of the archaeological Contractor's Project Design preparation. The document is available from the 'Archive Curator':

Dr L. Stewart, Curator of Archaeology and the Historic Environment,

Tel: 0151 478 4443

E-mail: liz.stewart@liverpoolmuseums.org.uk

Address: Urban History Division, NML, DTO, Albert Dock, Liverpool L3 4AX.

3.6 Other Matters

- 3.6.1 **Monitoring:** OA North will ensure that any significant results are brought to the attention of the Client and the Merseyside Archaeological Officer / Planning Officer as soon as is practically possible.
- 3.6.2 **Health and Safety:** full regard will, of course, be given to all constraints (services) during the survey, as well as to all Health and Safety considerations. The OA North Health and Safety Statement conforms to all the provisions of the SCAUM (Standing Conference of Unit Managers) Health and Safety manual. Risk assessments are undertaken as a matter of course for all projects. The Unit Safety Policy Statement will be provided to the Client, if required.
- 3.6.3 The buildings have sustained considerable water inundation and there are aspects of them that are unsafe. There are indications of unsound floors and there are considerable amounts of mould across the floors and walls which have released spores into the air. A comprehensive risk assessment will be undertaken by the OA North health and safety officer, who will advise on the appropriate action to ensure safe access to the building. It has been anticipated that, because of the risk of inhalation of fungal spores, that there will be a requirement for a decontamination unit, but the use of this will be subject to the risk assessment. Areas with unsafe floors will be marked off with barrier tape and will not be accessed. In some instances this may entail localised omissions within the survey data.
- 3.6.4 **Insurance:** the insurance in respect of claims for personal injury to or the death of any person under a contract of service with OA North and arising out of an in the course of such person's employment shall comply with the employers' liability (Compulsory Insurance) Act 1969 and any statutory orders made there under. For all other claims to cover the liability of OA North, in respect of personal injury or damage to property by negligence of OA North or any of its employees, there applies the insurance cover of £5m for any one occurrence or series of occurrences arising out of one event.
- 3.6.5 **Working Hours:** normal OA North working hours are between 9.00 am and 5.00 PM, Monday to Friday, though adjustments to hours may be made to maximise daylight working time in winter and to meet travel requirements. It is not normal practice for OA North staff to be asked to work weekends

or bank holidays and should the client require such time to be worked during the course of a project a contract variation to cover additional costs will be necessary.

- 3.6.6 **Confidentiality:** all internal reports to the client are designed as documents for the specific use of the Client, for the particular purpose as defined in the project brief and project design, and should be treated as such. They are not suitable for publication as academic documents or otherwise without amendment or revision.
- 3.6.7 **Staffing Resources:** the management of the project will be undertaken by **Jamie Quartermaine BA Hons** (OA North Senior Project Manager), to whom all correspondence should be addressed. Jamie has considerable experience of building surveys, having recorded many of the castles and abbeys in the region. The building survey will be undertaken by **Chris Wild BSc MIFA**.
- 3.6.8 The watching brief will be undertaken by **Caroline Raynor BA Hons AIFA** (OA North project officer). Caroline has a great deal of experience in the archaeology of Merseyside.

4 TIMETABLE

- 4.1 OA North can execute a project at very short notice, upon receipt of formal instruction. It is envisaged that the specified archaeological field work will require a two-week period to complete, although it is accepted that this may not necessarily be a continuous programme. The project brief divides the work programme into six main tasks, and the time required for each element may be broken down as follows:
- *Annotation of survey drawings:* two days on site
 - *Written description:* three days on site, undertaken simultaneously with drawing annotation
 - *Photographic record:* two days on site, undertaken simultaneously with drawing annotation and written description
 - *Report production:* fifteen days in office to produce report
- 4.2 The on-site work, comprising the annotation of architect's drawings, written description, and photographic record, could be completed within an 8 day period.
- 4.3 Once the fieldwork element has been completed, a final report can be compiled. A 25 day period should be allowed to compile the illustrations, including the annotated drawings, historic map regression, and relevant photographs.
- 4.4 The timetable for the watching brief is determined by the programme of the main contractor.

APPENDIX 2: CONTEXT LIST

Context Number	Context Location	Description
001	Area 1-5	Modern tarmac
002	Area 1 - 4	Granite cobble sett surface
003	Area 1 – 4	Black clinker bedding for setts 002
004	Area 1 – 3	Heterogeneous over burden layer
005	Area 1	East/west orientated hand-made red brick wall
006	Area 1	York stone slab – yard surface
007	Area 1	East/west orientated hand-made red brick wall
008	Area 1	North/south orientated hand-made red brick wall
009	Area 1	North/south orientated wall visible in west-facing section only
010	Area 1	Cellar/foundation infill of mixed clay and crushed demolition materials
011	Area 2	Mixed natural sandy brown clay – possibly redeposited natural
012	Area 2	Light-mid brown natural plastic sandy clay
013	Area 2	Cut of north/south orientated ditch
014	Area 2	Blackish-blue organic silty clay fill of 013
015	Area 3	Domed hand-made red brick well cap
016	Area 3	Vertical brick lined well shaft
017	Area 3	Construction cut for well 016
018	Area 3	Square brick plinth surrounding well
019	Area 3	Rusticated yellow sandstone foundations
020	Area 3	North/south orientated hand-made red brick wall built on top of 019
021	Area 3	East/west orientated row of large yellow sandstone kerb or edging blocks
022	Area 3	Red brick sailor row between sandstone edging 021 and 023
023	Area 3	Square granite stone tiles on east/west orientation parallel to 021 022
024	Area 4	Modern foundation wall
025	Area 4	Mixed rubble and concrete layer
026	Area 5	Continuation of granite setts
027	Area 5	Mixed orange clay deposit

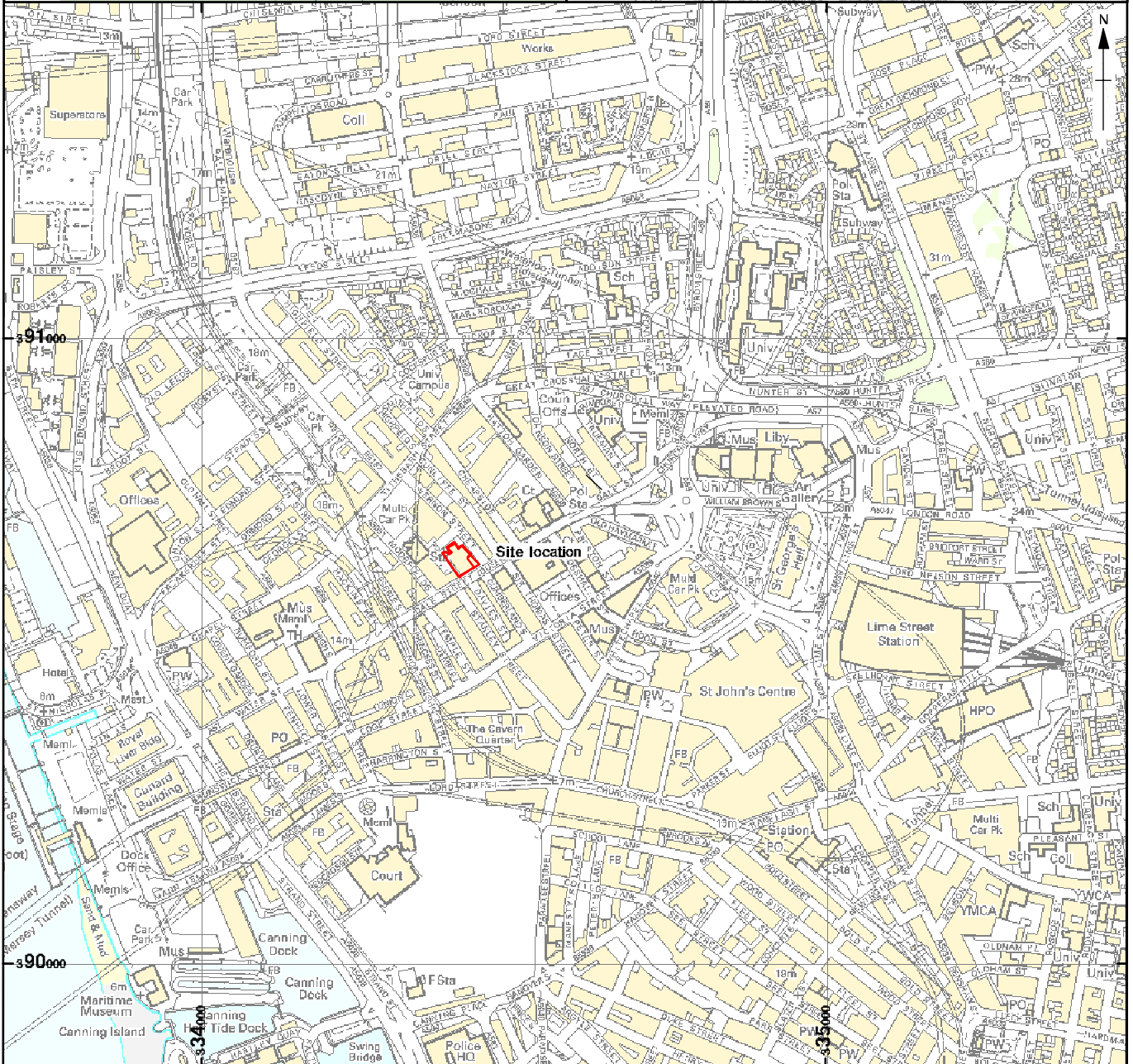
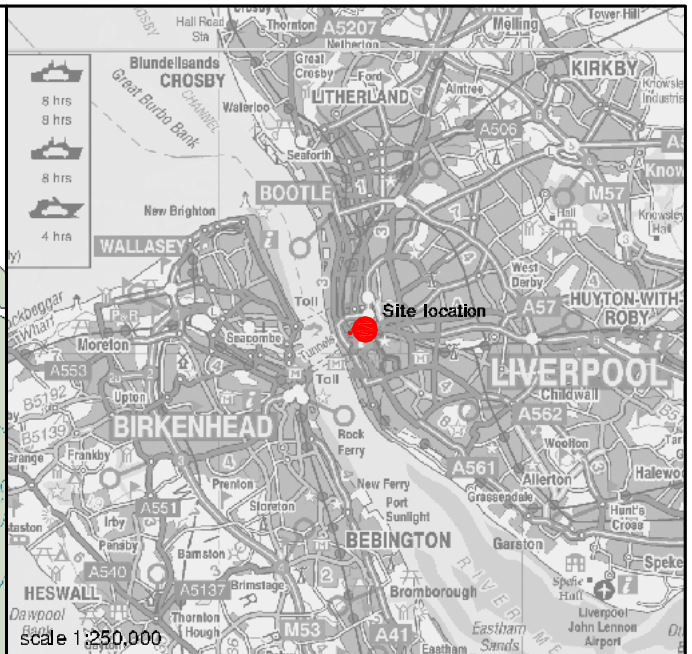
ILLUSTRATIONS

FIGURES

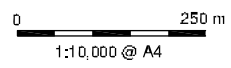
- Figure 1: Site Location Map
- Figure 2: The development area's location within the World Heritage Site and its relationship to the original 'H' street plan of Liverpool
- Figure 3: Okill's Reconstructed map of Liverpool of 1650
- Figure 4: Eyes' Map of Liverpool 1765
- Figure 5: Eyes' Map of Liverpool 1785
- Figure 6: Gage's Map of Liverpool 1807
- Figure 7: Ordnance Survey First Edition 6" to 1 mile, 1851
- Figure 8: Dower's Map of 1863
- Figure 9: Ordnance Survey 25" to 1 mile, 1890
- Figure 10: The exposed granite sett surfaces of former Hale Street in excavation Areas 1-5
- Figure 11: Excavation Areas 1 and 2 showing archaeological features
- Figure 12: Excavation Area 3 showing well and associated surfaces

PLATES

- Plate 1: Historic image of the area to the rear of the Hale Street Warehouse showing warehousing and derelict buildings prior to the area being cleared (LRO CE 11 555)
- Plate 2: South-facing view showing the excavation area in the foreground with ongoing demolition work of 57-65 Dale Street in the background
- Plate 3: Areas 1 and 2, east-facing shot of cobble sett road (part of Hale Street) **002**
- Plate 4: Area 1, south-facing shot of building foundations **006** and **008**
- Plate 5: Area 2, natural clay cut by probable boundary or drainage ditch **013/014**
- Plate 6: Area 3, capped well **015** and surrounding surfaces **018**, **021**, **022** and **023**
- Plate 7: East-facing view of wall **020** to the west of well cap **015** showing earlier sandstone foundations **019**
- Plate 8: Brick lined well shaft, **016**, following the removal of well cap **015**



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Figure 1: Site location

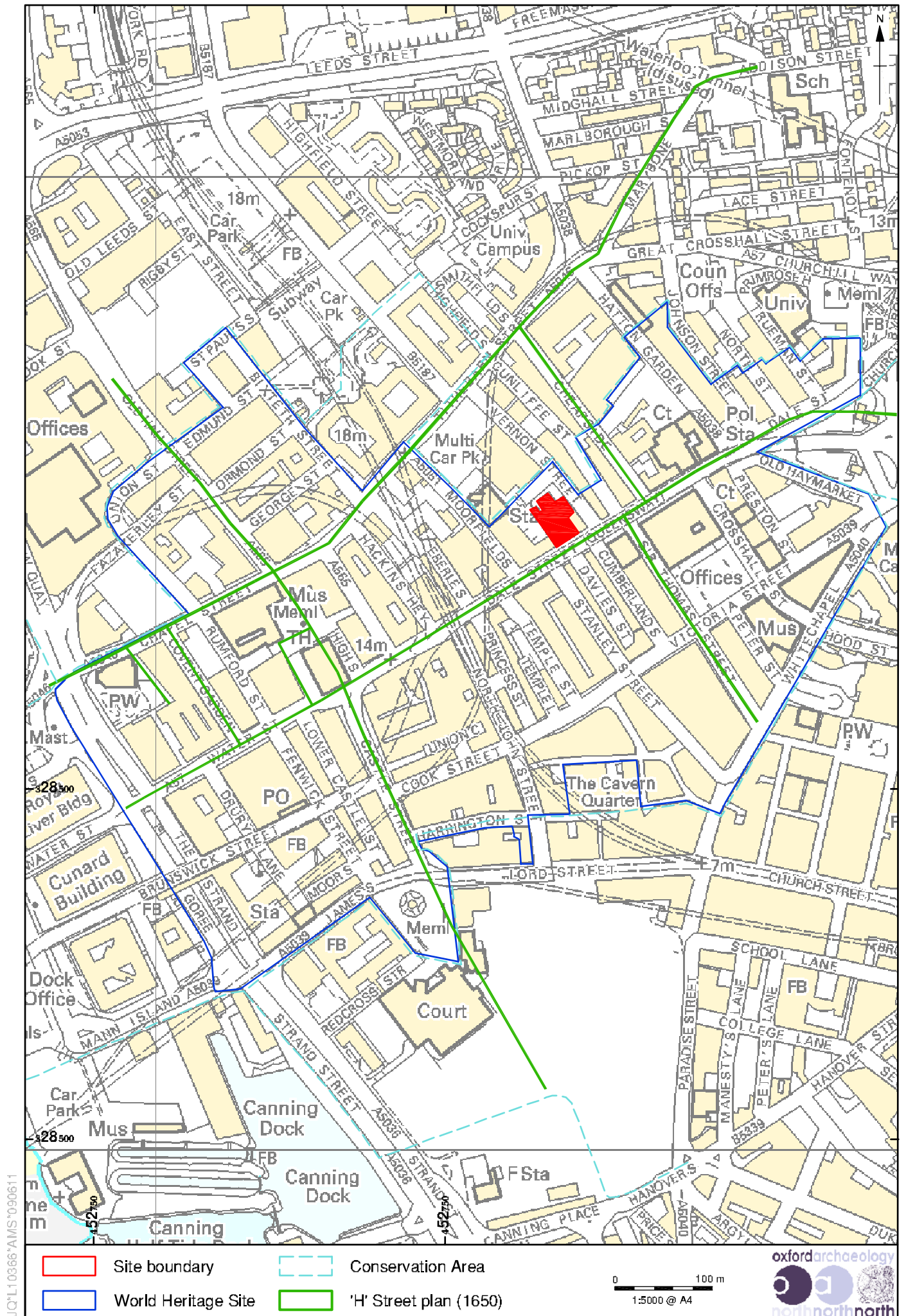


Figure 2: The development area's location within the World Heritage Site, and its relationship to the original 'H' Street plan of Liverpool



JQ/L1/0366*AMS*200511

Figure 3: Okill's Reconstructed map of Liverpool of 1650



JQ/L10366*AMS*200511

Figure 4: Eyes' Map of 1765



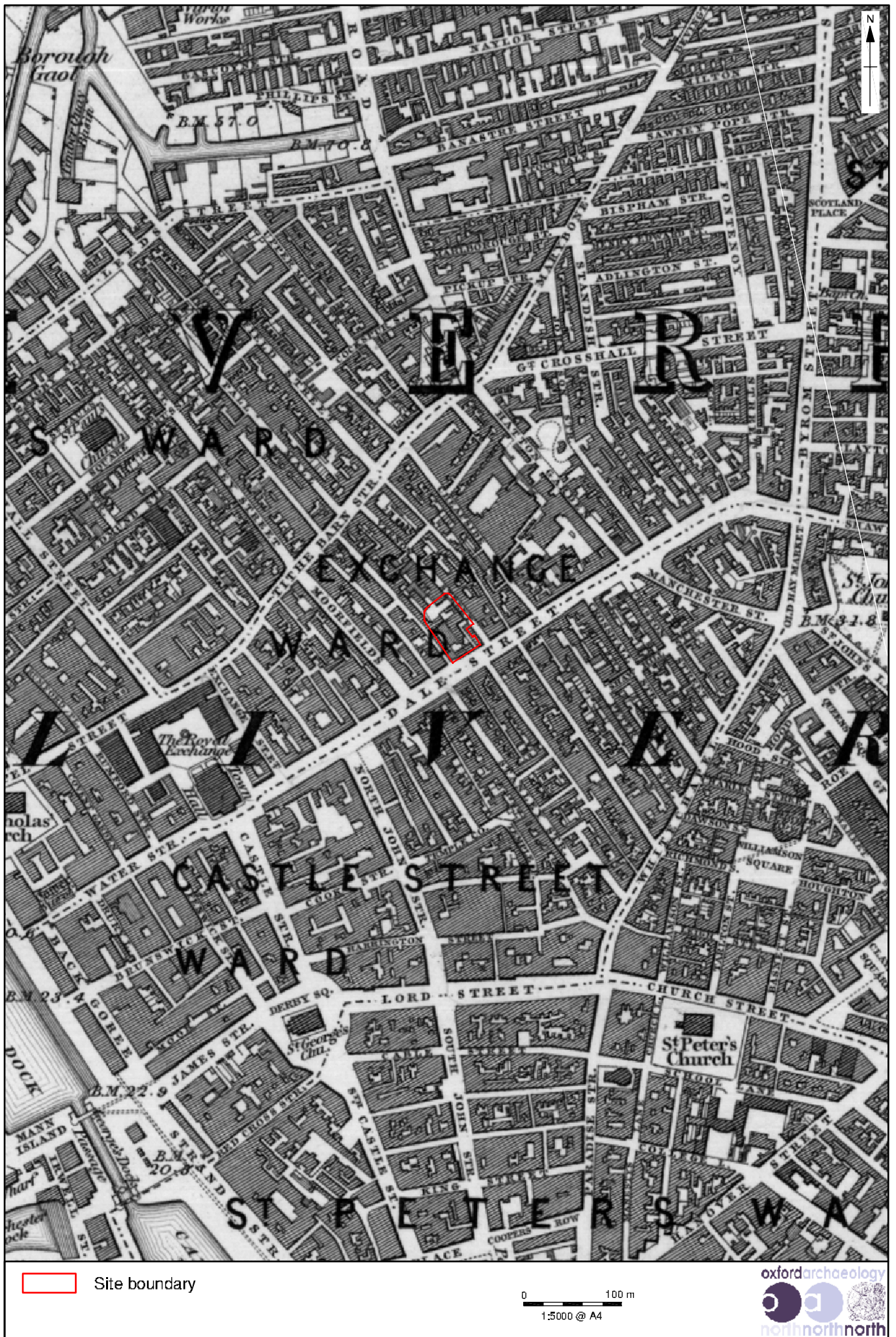
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Figure 5: Eyes' Map of Liverpool, 1785



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Figure 6: Gage's map of Liverpool, 1807



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Figure 7: Ordnance Survey first edition 6" to 1 mile, 1851



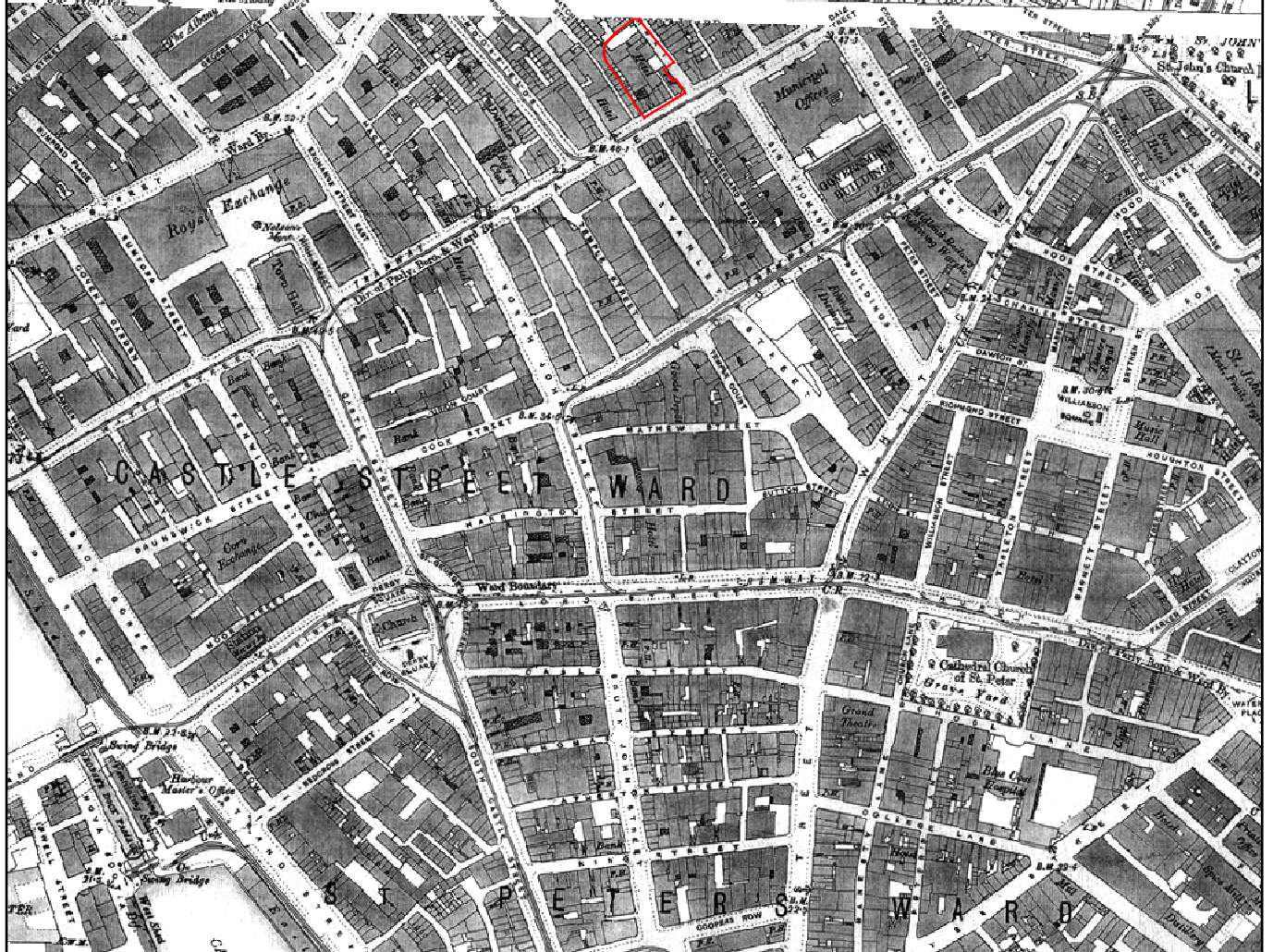
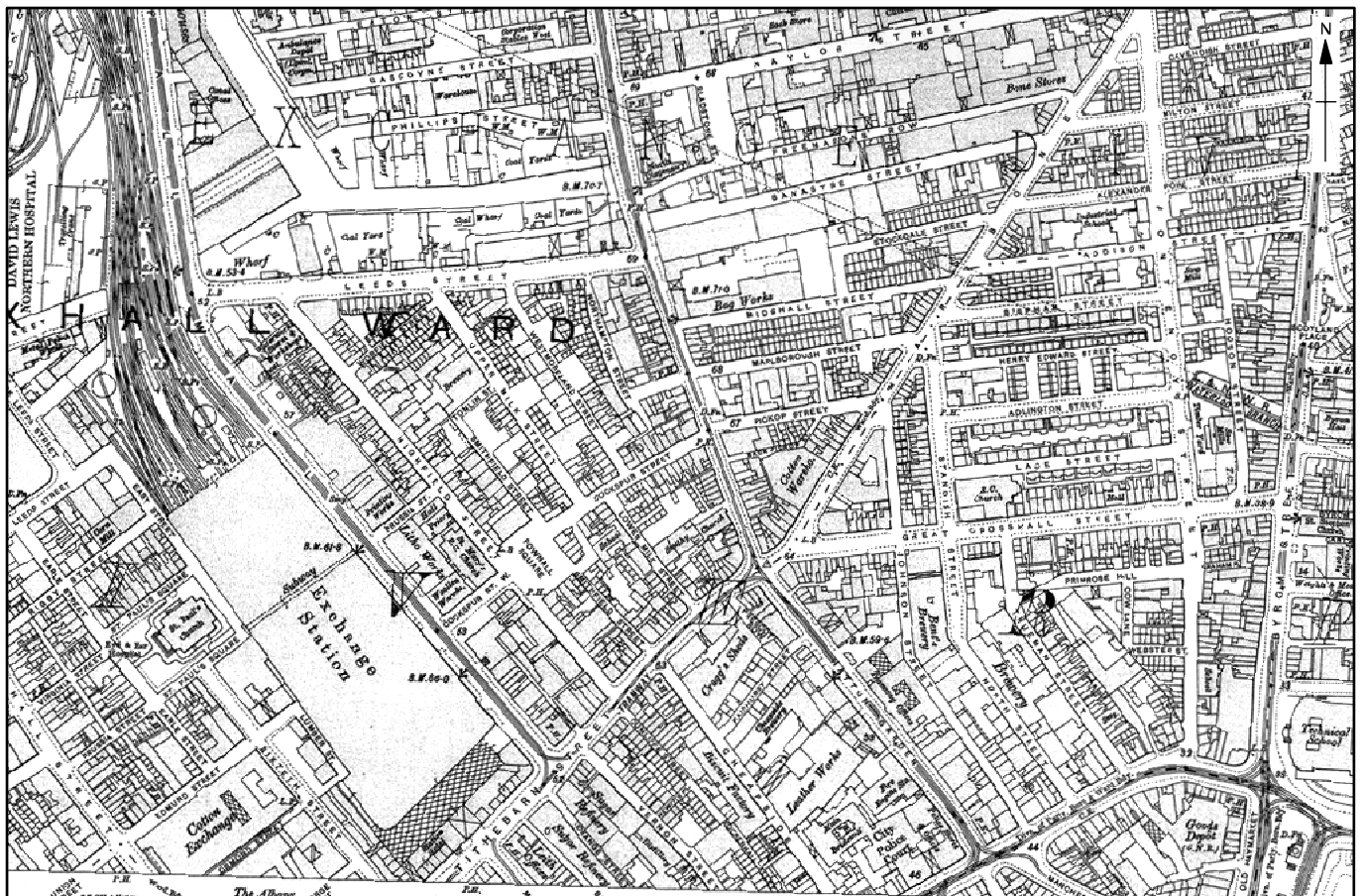
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 Site boundary

0 100 m
1:5000 @ A4



Figure 8: Dower's map of 1863



 Site boundary

0 100 m
1:5000 @ A4



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Figure 9: Ordnance Survey 25" to 1 mile, 1890

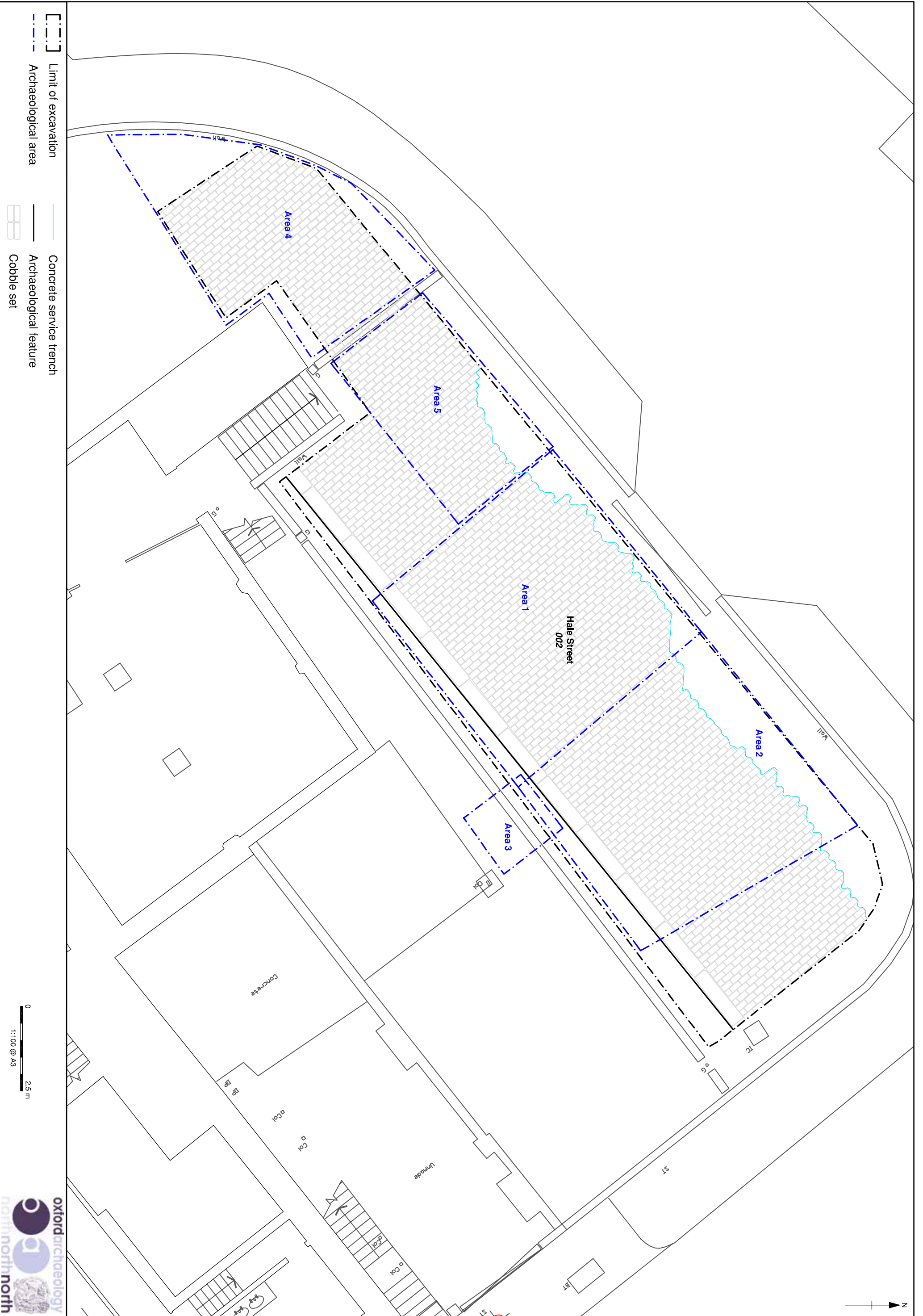


Figure 10: The exposed granite sett surfaces of former Hale Street in excavation Areas 1-5

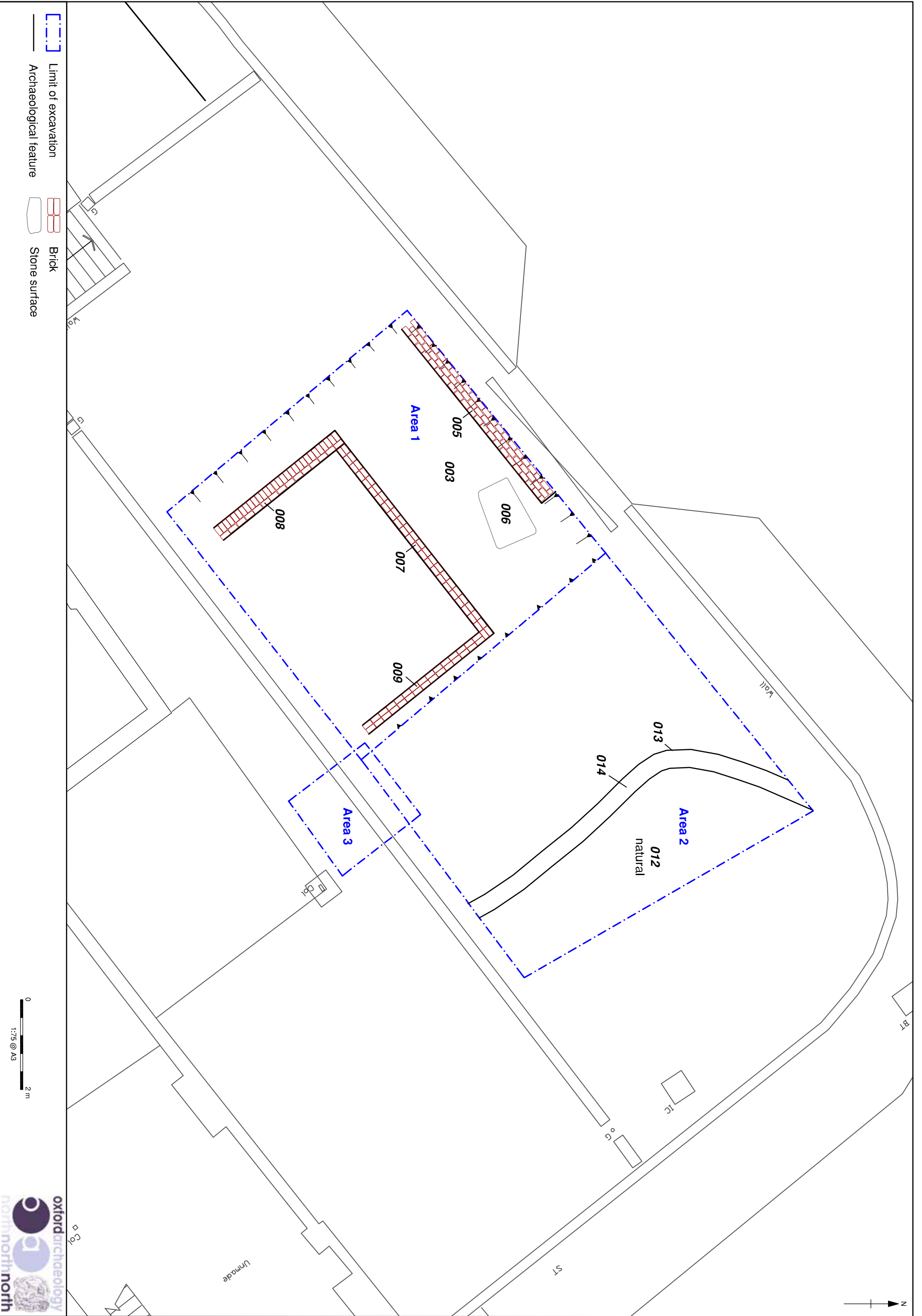


Figure 11: Excavation areas 1 and 2 showing archaeological features

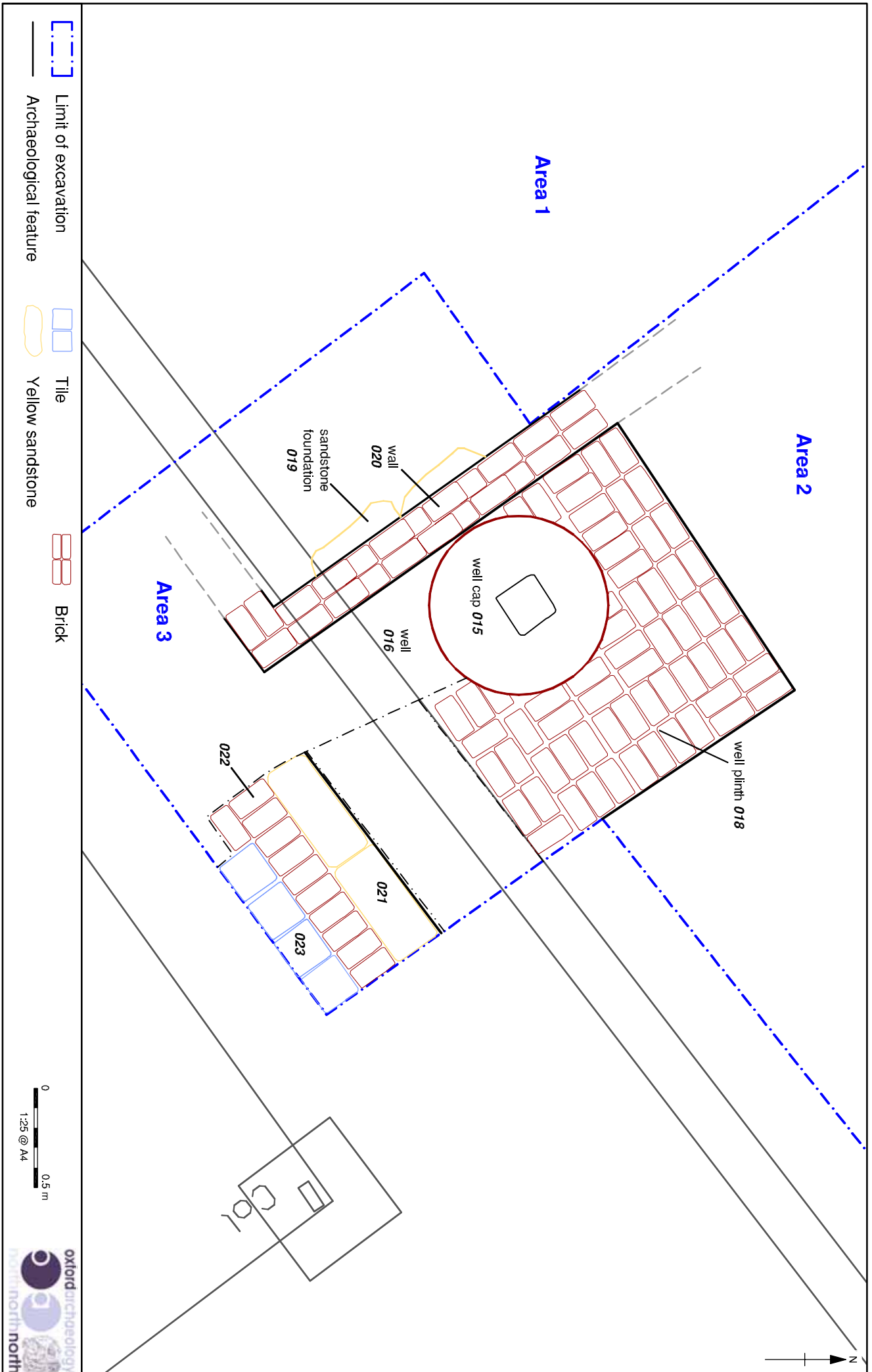


Figure 12: Excavation area 3 showing well and associated surfaces

PLATES



Plate 1: Historic image of the area to the rear of the Hale Street Warehouse showing warehousing and derelict buildings prior to the area being cleared (LRO CE 11 555)



Plate 2: South facing view showing the excavation area in the foreground with ongoing demolition work of 57-65 Dale Street in the background



Plate 3: Areas 1 and 2, east-facing shot of cobble sett road (part of Hale Street) **002**



Plate 4: Area 1, south-facing shot of building foundations **006** and **008**



Plate 5: Area 2, natural clay cut by probable boundary or drainage ditch
013/014



Plate 6: Area 3, capped well **015** and surrounding surfaces **018, 021, 022** and
023



Plate 7: East-facing view of wall **020** to the west of well cap **015** showing earlier sandstone foundations **019**



Plate 8: Brick-lined well shaft, **016**, following the removal of well cap **015**