



June 1999

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**BLACKBURN STATION**  
**LANCASHIRE**  
**Fabric Survey**

**Survey Report**

Blackburn Station  
Blackburn  
Lancashire

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Archaeological Fabric Survey Report

Report no 1997-98/059/AUA7889

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*'If, therefore, the Romans,  
a people decidedly belligerent, and almost strangers to peace,  
lavished such cost and labour upon their public way,  
how much more ought not we,  
who are fostered in the very bosom of peace,  
and tutored in the school of the arts and sciences,  
learn to estimate the benefits likely to result from railroads!'*

*- Buller, A Compendious History (1838)*



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## SUMMARY

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In April and May 1999, Lancaster University Archaeological Unit (LUAU) undertook an archaeological fabric survey and desk-top survey on Blackburn railway station (SD 685 279). This work was carried out on behalf of Oscar Faber, in advance of a redevelopment of the station, which will involve the demolition of the present buildings on platforms 1-5.

The desk-top study involved a rapid search of the pertinent available sources, examining historic maps and secondary sources to trace the development of the site. Copies of detailed design drawings from 1888 and a programme of refurbishment in the 1970s were provided by Oscar Faber.

The fabric survey was undertaken to RCHM(E) level 2 standard and involved the production of ground and first floor plans for the station buildings, in conjunction with a general and detail oblique photographic survey. As it was required that the southern, low level canopy over platforms 1, 2 and 3 should be reconstructed in a new location, an example of one of the trusses was recorded by rectified photography.

The desk-top study established that an earlier station, built on the site in 1847, had been entirely replaced by a new station in 1885-88. It is the later station which survives, largely intact, within the present structure, and this was confirmed by the fabric survey. The fabric survey identified a series of predominantly internal alterations to the buildings resultant from episodes of changing use, which occurred from the 1940s onwards, but mainly during the late 1970s and early 1980s.

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## ACKNOWLEDGEMENTS

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Lancaster University Archaeological Unit (LUAU) thanks Fred Arden of Oscar Faber for commissioning the project and for making available copies of archive drawings. Thanks also go to Dave Tildesley of Laings, John Taylor, and Colin Hart, the PICO's, for their cooperation and assistance during the fabric recording.

LUAU would also like to extend its thanks to Peter Iles of Lancashire Sites and Monuments Record, and to the staff of Lancashire Records Office and Blackburn Local Studies Library.

The desk-top survey was carried out by Jo Bell, the fieldwork was undertaken by Peter Redmayne and Neil Wearing. The CAD drawings were prepared by Neil Wearing, Mark Tidmarsh and Andrea Scott and the report was written by Peter Redmayne, and Jo Bell. The report was edited by Jamie Quartermaine and Rachel Newman and the project was managed by Jamie Quartermaine.

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# 1. INTRODUCTION

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## 1.1 CIRCUMSTANCES OF PROJECT

- 1.1.1 A combined desk-top and fabric survey was carried out by Lancaster University Archaeological Unit (LUAU), on behalf of Oscar Faber, prior to the demolition of all buildings on platforms 1-5 at Blackburn Station, Blackburn, Lancashire (SD 685 279) in April/May 1999. The survey provided for a mitigative record of the existing station buildings, in advance of their demolition, as required by a planning condition set by Blackburn with Darwen Borough Council. The survey comprised three main elements, a desk-top study, a photographic survey of the site, and a written and drawn record of the station buildings and associated subways.
- 1.1.2 The structure that contains the booking offices on the western side of the station is a Grade II listed building. This structure is to be retained, while the platform buildings and associated canopies are to be demolished, and the subways and cellars infilled.
- 1.1.3 In conjunction with the physical survey, a rapid desk-top survey of documentary evidence explored the historical development of the station. It was hoped that evidence would be recovered to explain what, if any, alterations had been made to the present railway station since its construction in the 1880s. It was also hoped that evidence might be found of the first railway station on the site, built *c*1847, and of its relationship to the present structure. Documents consulted included historic maps and plans, statutory records and published sources.
- 1.1.4 This report includes the results of the fabric survey, the desk-top study, and summary conclusions. A full archive of the survey has been produced to a professional standard in accordance with current IFA and English Heritage guidelines (*Management of Archaeological Projects*, 2nd edition 1991).

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## 2. METHODOLOGY

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### 2.1 PROJECT DESIGN

- 2.1.1 A project design (*Appendix 1*) was submitted by LUAU in response to a request from Oscar Faber for an archaeological survey of the Blackburn station platform buildings. It was designed in accordance with a verbal brief by Peter McCrone of the Lancashire County Archaeological Service (LCAS).
- 2.1.2 The project design provided for a desk-top study and a RCHM(E) level 2 fabric survey of the station. The fabric survey involved the creation of plans for all the platform buildings, a general oblique photographic survey of the extant structures, a rectified photographic survey of a truss of the low level canopy (platforms 1, 2 and 3) and an analytical descriptive record of the structures. Where practicable this project design was adhered to in full, and the work was consistent with the relevant standards and procedures of the Institute of Field Archaeologists, and generally accepted best practice. However, some buildings did not have safe access and were recorded on the basis of external observation only.
- 2.1.3 The results of the desk-top study and fabric survey have been integrated and are presented together within the present report.

### 2.2 DESK-TOP SURVEY

- 2.2.1 For the desk-top study, primary and secondary sources were consulted at a number of archives. Peter Iles of the Lancashire Sites and Monuments Record at Winckley House, Preston, was consulted to establish what information was available from this source. The Sites and Monuments Record is a database intended to include information on all known archaeological sites in the county. Listed Buildings records and aerial photographs were also examined here.
- 2.2.3 The County Record Office at Preston was visited, which has substantial archives on local railways, including correspondence, board meeting minutes for the various railway companies, plans and deeds. A number of documents which were requested for this study, notably deeds for the land on which the first station was built, were found to be very fragile and access to these was not allowed.
- 2.2.4 The Local Studies section of Blackburn Central Library, Town Hall Street, Blackburn was visited to consult historical maps of the study area, other primary material including pictures and photographs, and published sources. The library is currently scanning all its pictorial material for publication on the Internet, and no pictures were available which related to the study area.
- 2.2.5 Fred Arden (Oscar Faber) made available copies of original design drawings for the station, and some of these included twentieth century alterations.
- 2.2.6 LUAU's own library, and that of Lancaster University, were also used.

### 2.3 FABRIC SURVEY

- 2.3.1 The survey comprised two main elements, a comprehensive photographic survey of the site, and a written and drawn record of the station buildings and associated subways.
- 2.3.2 **Photographic Survey:** the photographic survey consisted of general oblique coverage of the site on 35mm black and white and colour print film. Additional photographs were taken of noteworthy architectural details (railings, clocks, etched glass, iron trusses and columns) and internal views of rooms where accessible.
- 2.3.3 Many of the buildings are derelict and badly vandalised, with evidence of squatting and drug use. Internal areas were recorded only where safe access was possible. In unsafe or inaccessible areas, inspection was done through external windows.
- 2.3.4 A truss above platform 3 was recorded in a series of rectified photographs using a medium format camera, to provide a record for possible future reconstruction. In the event it was not possible to provide a rectified photographic image of the very topmost part of the truss, because of obscuration by other trusses. However, the detailed original design drawings included measured drawing of both a truss (Fig 9) and a canopy column (Fig 10), which were observed to have sustained very little alteration since construction. It was agreed with the County Archaeologist that these original drawings, in conjunction with the rectified photographic record, would serve to provide for the reconstruction of the canopy.
- 2.3.5 **Drawn Survey:** the drawn survey involved the enhancement of existing survey drawings (supplied by Oscar Faber) using manual techniques. The base drawings comprised recently surveyed CAD drawings and also a series of plans and elevation drawings compiled in the 1970s/1980s as part of a programme of renovation. The early survey plans of individual station buildings were digitised into a CAD system (AutoCAD 14) and were incorporated with edits from the manual survey, which incorporated all variations in the building form subsequent to the production of the historic plans. These were superimposed with the modern outline CAD development map provided by Oscar Faber (Figs 4-6). A series of metric measurements at ground level as taken for each building to check the accuracies of the original plans, and to facilitate the transition from an imperial to a metric scale.
- 2.3.6 **Analysis:** a visual inspection of the site was undertaken and a general descriptive record was maintained of the structure utilising appropriate LUAU *pro-forma* record sheets to the Royal Commission on Historic Buildings in England (RCHME) level 2 standard. It involved the internal and external examination of the extant fabric, where health and safety allowed, and resulted in a description and assessment of the period, character and development of the buildings (*Section 4*).

## 2.4 ARCHIVE

- 2.4.1 A full archive of the desk-top study and the fabric survey has been produced in accordance with the current English Heritage guidelines (1991). The archive will be deposited with the LCRO and a copy of this report will also be deposited with the Lancashire Sites and Monuments Record.

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## 3. DESK-TOP SURVEY

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### 3.1 SOURCES

- 3.1.1 **Sites and Monuments Record (SMR):** the SMR includes an outline map showing the extent of Blackburn in the Middle Ages, and again in 1844. The study area lies

comfortably outside the boundaries of medieval Blackburn and clearly the town had expanded considerably prior to 1844. At that later date the study area was on the southern edge of the town, with no buildings to its south. The town has continued to grow since that date, and the station is now surrounded by housing and other buildings.

- 3.1.2 SMR record 10614 includes not only Blackburn station, but also the section of railway on which it lies, and various other stations and structures. It describes the establishment and growth of numerous local railways which later fell under the umbrella of the East Lancashire Railway, and later still the Lancashire and Yorkshire Railway company. Of the present station it says:

*'Blackburn station was originally built 1844-45 for the Bolton and Preston railway [sic]. The present station however is a rebuilding of 1885-88 and comprises a two storey brick built office with triple stone framed windows, stone parapet and clock; three aisle booking hall, subway and ramps to two long island platforms with two bays of North one of South [sic]. Twin train sheds with dividing wall and outside wall of yellow brick with twin semi-circular arched windows on town side, iron roof trusses, single and two storey yellow brick platform buildings. Goods station to south with long 3-aisle stone built shed' (SMR 10614).*

- 3.1.3 A further SMR record (10618) refers to the Bolton, Blackburn, Clitheroe and West Yorkshire Railway which operated several lines in the Blackburn area between 1847 and 1857, and built a second station in Blackburn at Bolton Road.

- 3.1.4 The SMR records no other sites of archaeological interest in the immediate vicinity of the station, with the exception of the All Hallows Spring Well (SMR 0978). The well lay c70m north of the station, and in 1906 still existed 'under premises in Station Road, and with a most abundant supply of water' (SMR 0978). From the Middle Ages the water was said to be good for sore eyes, and in 1654 a Roman inscribed stone was found near the well.

- 3.1.5 Aerial photographs of the area were also examined at the SMR Office. Black and white oblique photographs (N.1308 and 1524/53) show the station as a long structure with twin-roofed train sheds, which is clearly the present building; however, little other information can be gleaned from these photographs.

- 3.1.6 Listed Building Record SD 6827: 796-/2/10018 was also examined at the SMR Office. The railway station was Listed Grade II in September 1995. The Listing refers to a:

*'Railway station booking hall.....c1886-88, for the Lancashire and Yorkshire Railway Company, replacing an earlier and smaller station building; altered, and the main floor now unoccupied. Red brick in Flemish bond with fine penny-struck pointing, sandstone dressings, hipped slate roof to main block, glazed roofs on iron framework to front canopy and to side and rear ranges.'*

- 3.1.7 It describes the glazed canopies, the large clock above the centre of the booking hall facade, and 'four tall panelled and corniced chimney stacks' as well as other details which clearly identify the present building.

- 3.1.8 **Lancashire Record Office:** at the County Record Office, the first available map of the area was the first edition Ordnance Survey map (1848), Lancashire Sheet 62, at a scale of 6" to the mile (Fig 2). The map was surveyed in 1846 and does not show the railway or the station, although both were probably under construction at the time. The site on which the station was to be built is shown at this date as a space between the river Blakewater to its west, and the Leeds and Liverpool Canal to its east. The space included a bowling green immediately to the south-west of Mount Street, several private gardens or allotments and a few small fields. It is clear that the site was on the periphery of the town, which was essentially a linear settlement aligned south-west to north-east (between Whalley Banks and Eanam roads), with a spur running south along Darwen Street to the canal. Industry

was clearly established in Blackburn by this date, and sites near the future station included coal wharves, gas works, foundries and mills producing both cotton and woollen goods.

- 3.1.9 A close study was made of transcribed minutes of board meetings at the various local railway companies. No structural details or plans were available for the first railway station on the Blackburn site; however, it was found that the contract for building the station was awarded on 19 November 1845 to Stones and Nephew of Blackburn. The contract for building the roof was awarded separately to Ireland and Co. of Manchester (CRO DDX/472/4). 'William Stones builder' and 'Richard Hacking mason' were also granted the tender to build the goods warehouse, to be completed by January 1847. None of these individuals or companies deposited records with the County Record Office, and it is not known whether their documents survive in any other archive.
- 3.1.10 Further transcripts at the County Record Office (CRO DDX/472/4) show that the tender for a new station, *ie* the present building, was granted on 18 February 1885 to R Neill and Sons.
- 3.1.11 **Blackburn Library:** at Blackburn Library's Local Studies section, the Ordnance Survey's second edition map of 1894, Lancashire Sheet 62.16, was viewed; this is the earliest map available at the larger scale of 25" (Fig 3). This map shows the present railway station in place, with its plan essentially as it remains, a twin train-shed aligned south-west to north-east. The new markets and changing society brought by the railway are in evidence on this map. The industries represented near the station include dye- and chemical works, engineering works, a saw mill and a brewery. All the mills in the area of the station were cotton mills by 1894, contrasting with the mixed woollen and cotton industries of 1846. A massive expansion of back-to-back housing, accommodating mill and other workers, had taken place on the east bank of the canal, which had been vacant in 1846.
- 3.1.12 **Public Record Office:** the Public Records Office (PRO) at Kew was cited in the project design (*Appendix 1*) as a possible source of material; however, in the event the design drawings supplied by the client, and those records held by the County Records Office, provided all the information for which a visit to the PRO had been intended. Scrutiny of the extensive PRO web site and e-mail correspondence with the PRO indicated that no further information would be gained by a visit to the PRO, hence it was decided to omit such a visit to the PRO.
- 3.1.13 No large-scale map was available within any archive for any date between 1848 and 1894 and it was therefore impossible to examine the plan of the first railway station, which stood on the site between 1847 and 1888.

## 3.2 HISTORY OF BLACKBURN STATION

- 3.2.1 The site on which the railway station stands was probably never built on before the mid-1840s, as the Ordnance Survey mapping (6" 1848, Lancashire Sheet 62) shows gardens and fields in the area. At this time the town was already industrial in character, and clearly ideal for connection by railway with new markets and suppliers. Coal mines and wharves, foundries and flour mills were to be found close to the site, as were cotton and woollen mills. These were generally clustered on the banks of the river Blakewater, which had formerly supplied power to many of the mills, and also on the banks of the Leeds and Liverpool Canal, whose arrival in 1816 had established markets and communications structures for the manufacturers of Blackburn.

- 3.2.2 The first railway station on the present site was built in 1846, for the Blackburn and Preston Railway Company. The Ordnance Survey map surveyed in that year does not show the station or railway, but they were certainly under construction. Royal Assent was granted for the line to Blackburn station in July 1845, and a station was commissioned in late 1845 from a local building company, Stones and Nephew of Blackburn. The roof, which included glazed cast iron frames and therefore was presumably outside the experience of Stones and Nephew, was built by Ireland and Co. of Manchester. Little is known of this station; it has been described as '*a two-storey building with two wings*' (Holt 1978, 193), but this description may in fact refer to another station at Bolton Road (Section 3.2.4), which, in an 1848 illustration fits this description exactly (Blackburn Library, ref N34/4646).
- 3.2.3 The line to Preston, and the station in Blackburn, were officially opened on 3 June 1846 (CRO DDX/472/4). The goods warehouse, to be completed by January 1847, was built by the partnership of William Stones, builder (of the same company who had built the station) and Richard Hacking, mason (*ibid*).
- 3.2.4 At least two other stations existed in the town by 1847, at Bolton Road and Daisy Hill. This was a result of separate campaigns by various private railway companies, each building sections of railway terminating at their own station. In the case of the Bolton Road station, competition led to failure: the station, built for the Bolton, Blackburn, Clitheroe and West Yorkshire Railway, was closed after only a year. In the following decade, several of the smaller companies were amalgamated into the East Lancashire Railway or the Lancashire and Yorkshire Railway (SMR, PRN 10614). In 1859 the Lancashire and Yorkshire Railway (LYR) absorbed its rival, and it was LYR which centralised operations and commissioned a new railway station for Blackburn in 1885.
- 3.2.6 The textile industry in Blackburn had boomed in the decades since the first station was built. By 1867 there were 107 cotton spinning mills in and around Blackburn (Holt 1978, 193). The expanding textile industry and the extending railway network were mutually beneficial, and the town grew very rapidly as '*the mill building boom of 1849 to 1870 partnered railway growth*' (Beattie 1992, 26). The old railway station was probably unequal to the increased volume of goods and passenger traffic wishing to use it and so LYR decided to replace it. On 18 February 1885 the company accepted the tender of R Neill and Sons to build '*a new passenger station at Blackburn, excluding glazing*' for the sum of £53,232.16s.6d. It was to be built on the site of the earlier station, which was apparently completely demolished.
- 3.2.7 The new station was completed in 1888. It was built in an Italianate style which '*was almost old-fashioned by the time it was complete*' (Biddle and Nock 1983, 107). These buildings survive almost intact within the present railway station, although the two-storey booking hall area to the front has had the ground floor altered, and the other station buildings have undergone repeated episodes of mainly internal alteration.
- 3.2.8 Architects' drawings, supplied by the client, show the original design plan to have been for three main buildings on each platform. Towards the northern end were large buildings containing general waiting rooms and mens' toilets (Buildings 3, and 8), to the north of the two subway ramps were smaller buildings divided into ladies first and second class waiting rooms with separate toilets for each class in the centre (Buildings 2, and 7).
- 3.2.9 To the south of the ramps were two longer buildings (Buildings 1, and 6). Building 6 contained a second class refreshment room at its northern end with a first class refreshment

room dining room and gentleman's first class waiting room to the south. A smaller building to the south (Building 5) contained a lamp room and guards room.

- 3.2.10 Building 3 had a second class refreshment room at its northern end with a gentleman's first class waiting room immediately to the south. The rest of the building contained a telegraph office, station master's, porters' and inspector's rooms.
- 3.2.11 Repeated episodes of alteration to station facilities were carried out roughly along the following lines: in the 1940s and 1950s the toilets and accommodation for staff and passengers were updated, for example by the substitution of electric lighting for gas lamps. In 1962 the enquiry office was refurbished with new doors, panelling and flooring. This particular programme of alterations may have been prompted by a fire, as architects' drawings require a burnt portion of partition wall to be replaced with asbestos insulation board. In 1972, the ticket office was refurbished and 'lineman accommodation' added. Further substantial reorganisation and replacement of the ticket office was undertaken in 1982, and it was at this date that a new façade was built for the ground floor.
- 3.2.12 With the exception of the 1982 programme, all changes seem to have been relatively superficial adaptations of function and internal division, rather than substantial alterations to the structure of the station. The station as it stands today is largely a survival of the 1880s, a time at which the cotton town of Blackburn, and the railway networks serving it, were at the height of their powers.

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## 4. FABRIC SURVEY

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### 4.1 STATION BUILDINGS

- 4.1.1 The buildings were numbered from 1-9 for ease of reference. Buildings 1-4 (Fig 4) were on platforms 4 and 5, and Buildings 5-8 were on platforms 1 and 2.
- 4.1.2 The station buildings on both platforms vary somewhat in size, but have been constructed to a uniform design, with pale brown sandstone plinths, and cream/white glazed brick on the exterior. The buildings had wide, symmetrically positioned, openings filled at the base with two or three wooden panelled sections. Each section was divided by projecting vertical timber struts with a moulding to the outer face.
- 4.1.3 The upper parts of the openings were filled with large glazed lights containing some original acid etched panes with a decorative greek key style border. Some panes had lettering denoting the original functions of individual rooms within the buildings, such as dining and waiting rooms.
- 4.1.4 In some cases one section of the opening contained a door, set either in the central or side divisions of the panelling, and others functioned merely as large windows. The height of the etched panes would have provided privacy for people inside the buildings, as they extended above normal eye level.
- 4.1.5 All buildings were single storey with the exception of the large two storey building (Building 6) between platforms 1 and 2.

### 4.2 BUILDING 1

- 4.2.1 Building 1 (Fig 6) was a four bay structure at the southern end of platforms 4 and 5. The windows to the first (southern) bay retained some etched glass with the text 'general waiting room'. In the second bay the glass was etched with 'waiting room', and in the third 'refreshment room'; no original glass survived in the fourth (northern) bay.
- 4.2.2 The internal plan of the building had been lost due to extensive modern partitioning, and only partial access was possible. Some original features such as oak wainscot panelling remained *in situ*.

### 4.3 BUILDING 2

- 4.3.1 Building 2 (Fig 5) was a two bay structure to the north-east of Building 1, the two buildings being divided by the subway access. Only a single pane of etched glass survived in the door to the northern bay which had 'LAD' above '1st & 2nd', the second pane had been replaced but probably read 'IES' above 'CLASS' (Plate 7). Internally the building was divided into two parts by a modern ticket desk.

### 4.4 BUILDING 3

4.4.1 Building 3 (Fig 5) was a three bay building situated at the northern end of platforms 4 and 5. Most of the window/door openings retained plain etched glass panes. The building was internally divided into five rooms; however, one of the walls was a modern stud partition, and the building was probably originally divided into four rooms (Plate 8). In the south-eastern corner of the narrower central bay was a large stone fireplace (Plate 9). Most of the walls retained original wainscot panelling.

#### 4.5 BUILDING 4

4.5.1 Building 4 (Fig 5) was a small windowless building at the northern end of platforms 4 and 5. The northern part contained a male toilet block while the southern part appeared to have been a workshop; there was also a small single toilet inserted into the south-western corner.

#### 4.6 BUILDING 5

4.6.1 Building 5 (Fig 6) had a small wooden panelled structure which was added to the southern end of the original 1888 building on platforms 1 and 2. There was a window in the south wall and a door in the east wall.

4.6.2 The main room was a two bay structure with two doors in the east elevation opening to a single room. There were two windows in the west wall above the closed-off subway on platform 1. As in other buildings some original panelling survived, but it was not possible to tell whether it had originally been divided into one or two rooms.

#### 4.7 BUILDING 6

4.7.1 The ground floor of this building (Fig 6) was a five bay structure with a second floor lit with a series of five broad low arched windows with red brick arched surrounds.

4.7.2 This building retains mostly original features, particularly on the second floor, where all rooms had panelled doors, deep skirting boards, and in some places moulded ceiling cornices. The original function of the ground floor was as refreshment/dining rooms, as evidenced by panels of etched glass in one of the windows in the west elevation. There is a dumb waiter and speaking tube connecting to a large room at the northern end of the first floor, which was probably the original kitchen (Plates 10 and 11).

4.7.3 In the northern ground floor room were the remains of a moulded cornice, and, instead of wainscot panelling, there were brown glazed tiles between the dado rail and skirting, set in a diamond pattern, with an upper band of diamond pattern white and brown glazed tiles.

4.7.4 The floors of the former dining room area (Plate 6) were covered with a pattern of coloured linoleum sections, which may have been the original floor.

4.7.5 Most of the upstairs rooms (Fig 7) appeared to have been used as staff accommodation, which were accessed from a passage on the east side of the building. There were also two smaller rooms near the head of the stairs which may have functioned as toilet/washrooms, and a large room at the northern end (*Section 4.7.2*).

4.7.6 There was a cellar running the full length of the building which had two blocked off barrel drops in the east wall, with a smaller cellar at the northern end. This cellar was linked, at its southern end, to the subway which connected platforms 1 and 2 to a large building on the south-western side of the station.

## 4.8 BUILDING 7

- 4.8.1 Building 7 (Fig 5) was a two bay structure, and was still in use as railway offices (Plate 4). There were two large station clocks attached to the east and west elevations at the southern end of the building. The clocks were attached to the wall with substantial iron brackets with decorative scrolled ironwork between; they were originally driven by a weight-operated mechanism contained within the southern end of the building, but had later been converted to electric power. Both mechanisms were worked from a single horizontal spindle, which ensured both clocks always showed the same time (Plate 5).

## 4.9 BUILDING 8

- 4.9.1 Building 8 (Fig 5) comprised a two bay structure with a later extension to the north. No original etched glass survived and the lower windows were boarded up. The extension to the north had no windows but was built in a similar style to the other station buildings.

## 4.10 BUILDING 9

- 4.10.1 Building 9 (Fig 5) was a small square windowless building with a door in the south wall. Access was not possible due to the door being boarded up and the outer walls were obscured by scaffolding which was piled up against them.

## 4.11 THE CANOPIES

- 4.11.1 The two high canopies above platforms 1, 2, and 4 were supported on a central spine wall, above the east walls of the station buildings on platforms 1 and 2. Where there were gaps between the buildings the canopies were carried on substantial girders. The outer side of the eastern canopy was supported on the west walls of the buildings on platforms 4, and 5, while the western canopy is supported by a continuous side wall with a series of high arched openings. The wall on the western side has a series of high arched openings and at its northern end forms the outer wall of the station.
- 4.11.2 The canopies are glazed, and constructed with a series of cross-braced iron girders with small decorative motifs at the crossing point of the braces (Plate 2). The central sections project above the main canopy forming smoke hoods, the tops of which are supported on arched ironwork girders. There were also glazed wind screens across the gable ends of the canopies.
- 4.11.3 Above platform 3 is a low level glazed canopy constructed with pairs of cast iron columns carrying supports for cross-braced rectangular box-section girders with sloping triangular terminals (Figs 9 and 10) (Plate 2). The girder supports have decorative spandrels pierced with a six lobed design round a central pierced hole, with smaller pierced holes between each pair of lobes (Plate 12). Each cross brace of the box section girders has an example of a four petalled flower which was the emblem of the Lancashire and Yorkshire Railway Company.
- 4.11.4 The outer edges of this canopy extend over the southern part of platforms 2 and 5 to provide shelter outside the coverage of the main canopies. There is a similar part canopy attached to the facade of the station entrance, also with the floral motif, and a second part

canopy of similar design which extended along the length of platform 4. The edges of these canopies were decorated with a continuous fretted wood valence.

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## 5. CONCLUSIONS

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### 5.1 1840S STATION

- 5.1.1 The site was not built on before the late 1840s, when the first railway station was constructed by the Blackburn and Preston Railway company. Nothing is yet known about the form or style of this building, and no evidence was recovered of it during the fabric or desk-top surveys. It is believed that the earlier station was entirely replaced by the present one. This existing building was commissioned in 1885 by the Lancashire and Yorkshire Railway company and completed in 1888.

### 5.2 1885-8 STATION

- 5.2.1 The existing structures are substantially those of the 1888 building, which were constructed in a single build. The station booking hall is Listed Grade II, but the station buildings are not generally considered an outstanding example of railway architecture. Alterations to the station have been generally confined to internal structural arrangements in passenger and staff accommodation, undertaken at various dates from the 1940s onwards. Physical evidence of these alterations includes modern partition walls, and wall and floor coverings. The most substantial change to the structure was the creation of a new ground-floor facade to the ticket and booking offices in the 1970s and 1980s (Plate 1) (*Section 3.2.1*).

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## 6. BIBLIOGRAPHY

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### 6.1 PRIMARY SOURCES

#### 6.1.1 *Lancashire Record Office*

DDX/472/4 Various transcripts of railway company minutes

#### 6.1.2 *Listed Building Record*

SD 6827 NW: 796-/2/10018, *Blackburn Railway Station*

#### 6.1.3 AERIAL PHOTOGRAPHS

B/W photos N.1308 and 1524/53 (at SMR)

#### 6.1.4 *Sites and Monuments Record*

PRN 0978, Allhallows Spring Well

PRN 10614, East Lancashire Railway (Blackburn and Preston Railway)

PRN 10618, Bolton, Blackburn, Clitheroe and West Yorkshire Railway

#### 6.1.5 ORDNANCE SURVEY MAPS

1848 (First Edition) 6" to 1 mile, Lancs 62

1894 (Second Edition) 25" to 1 mile, Lancs 62.16

1911 (Third Edition) 25" to 1 mile, Lancs 62.16

### 6.2 SECONDARY SOURCES

Association of County Archaeological Officers (ACAO), 1993 *Model briefs and specifications for Archaeological Assessments and Field Evaluations*, Bedford

Beattie, D, 1992 *Blackburn: the Development of A Lancashire Cotton Town*, Halifax

Biddle, G, and Nock, OS, 1983 *The Railway Heritage of Britain*, London

Buller, E, 1838 *A Compendious History and Description of the Northern Union Railway*, Preston

Carlson, RE, 1969 *The Liverpool and Manchester Railway Project*, unknown

Durham, W, 1861 *Chronological Notes of Blackburn, Lancashire*, Blackburn

Harrison, W, 1882 *History of the Manchester Railways* (2nd edn, 1967), Manchester

Holt, GO, 1978 *A Regional History of the Railways of Great Britain: 10, the North West*, London

Institute of Field Archaeologists (IFA), 1992 *Guidelines for data collection and compilation*, London

Liverpool and Manchester Railway, 1930 *Centenary Celebrations*, Liverpool

Marshall, J, 1962 Manchester to Hellifield - 2, *The Railway Magazine*, November 1962

Tattersall, WD, 1973 *The Bolton, Blackburn, Clitheroe and West Yorkshire Railway*, London

Ward, C, 1976 *Blackburn via the Iron Road: an account of the development of the railway and its effects upon Blackburn from 1846-1901*, unpubl rep

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APPENDIX 1  
PROJECT DESIGN

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Lancaster  
University  
Archaeological  
Unit

April 1999

**BLACKBURN STATION,  
LANCASHIRE**

**ARCHAEOLOGICAL FABRIC SURVEY**

**Proposals**

*The following design is offered in response to a request from Mr Arden of Oscar Faber for an archaeological fabric survey as a planning condition for the refurbishment of Blackburn Railway Station, Blackburn, Lancashire.*

## **1. INTRODUCTION**

### **1.1 PROJECT BACKGROUND**

1.1.1 Mr Fred Arden, of Oscar Faber, has requested that Lancaster University Archaeological Unit submit proposals for a fabric survey at Blackburn Railway Station, Lancashire, in advance of the refurbishment of the station.

1.1.2 The station is a grade II listed building and the archaeological condition has been applied to the present planning application by the Blackburn with Darwen Borough Council, in line with PPG 15 (Section 2.15), because of the archaeological potential of the structures. The Blackburn station buildings were constructed in the 1880's replacing earlier station buildings of the Lancashire and Yorkshire Railway Company. The complex comprises a mainly single storey range, apart from the booking hall which is two storey. It has a large deep canopy overlying the station buildings which is supported by cast iron columns forming five wide bays, with wrought iron trusses.

### **1.2 LANCASTER UNIVERSITY ARCHAEOLOGICAL UNIT**

1.2.1 LUAU has considerable experience of fabric survey of sites of all periods, having undertaken a great number of small- and large-scale projects during the past 18 years. LUAU has the professional expertise and resource to undertake the project detailed below to a high level of quality and efficiency. LUAU and all its members of staff operate subject to the Institute of Field Archaeologists (IFA) Code of Conduct. LUAU has considerable experience of the recording of industrial buildings, in particular the First White Cloth Hall in Leeds commissioned by English Heritage, the Backbarrow Ironworks in South Cumbria, Hatton Hospital, a Victorian hospital in Warwickshire, Hotties glass Works in St Helens and also an ongoing project at Wigmore Castle (Hereford) also for English Heritage. Other recent building recording projects include a number of churches and public houses throughout Lancashire.

## **2. OBJECTIVES**

2.1 The following programme has been designed, in accordance with a verbal brief from Peter McCrone of Lancashire County Archaeological Service, to provide a fabric survey of the station buildings. The required stages to achieve these ends are as follows:

### **2.2 DESK-TOP STUDY**

2.2.1 To undertake historical research, using primary sources to establish the history of the buildings and their development.

### **2.3 SITE SURVEY**

2.3.1 Ground plans will be drawn for the station buildings that will be affected by the development, and a general site plan will be generated. These will be based on surveys to be provided by the client.

### **2.4 PHOTOGRAPHIC SURVEY**

2.4.1 To provide a general photographic record of the station buildings and also representative examples of architectural detail, which will include the wrought iron trusses of the canopy roof. This will be in accordance with the Royal Commission on the Historical Monuments (England) (RCHME) Level 2 survey.

### **2.5 DESCRIPTION AND ANALYSIS**

2.5.1 This would provide a basic investigation of the fabric and would result in a textual description of the station fabric, coupled with an oblique photographic record of the internal / external elevations of the station building.

### **2.5 SURVEY REPORT**

2.5.1 A written survey report will assess the significance of the data generated by this programme within a local and regional context. It will present the survey results and would make an assessment of the form of the station buildings.

## **3. METHOD STATEMENT**

### **3.1 DESK-TOP STUDY**

- 3.1.1 The work will rapidly assess the full range of potential sources of information. It will include an appraisal of the relevant Sites and Monuments Record, the listed building records as well as appropriate sections of British Rail and architectural records as may reasonably be available. Any photographic material lodged in either the relevant Sites and Monuments Record or the relevant Record Offices will also be studied. However, the records for British Rail are held in the Public Record Office and therefore will require a visit to London. Published documentary sources such as the relevant volume of Pevsner's Buildings of England series and any other volumes of railway architecture available will be examined and assessed. A visit to the Blackburn reference library will be undertaken to examine secondary sources. This work may involve visits to the County Record Office in Preston, subject to a telephone search.
- 3.1.2 The study will examine all possible cartographic sources, which would include early maps, and such primary documentation as may be reasonably available. The study will attempt to find the original cartographic proposals and designs for the station. Any photographic material lodged in either the County Sites and Monuments Record or the various County Record Offices will also be studied. Published documentary sources will also be examined and assessed.
- 3.1.3 The study will investigate the history of the building and will investigate, from primary or secondary sources, as appropriate the context for the construction of the station, the architects and builders. It will seek to identify the development and the changing use of the station buildings.

### **3.2 SITE SURVEY**

- 3.2.1 A series of plans for each floor of the station will be created and will be based on the architects plans to be provided by the client. The ground plan for the station is available within a CAD format and will be edited directly within that format. The other plans are in a hard copy format and will be digitised into a CAD system. The survey will create three plans: a sub-platform plan, platform plan and a second storey plan.
- 3.2.2 The base drawings are to an extent schematic, in terms of representation of detail but are spatially accurate. The survey will involve the addition of significant architectural detail onto the base plan, which will be undertaken using manual survey techniques, and these edits will be digitised into the CAD system. The survey will record all significant, extant structural elements but will not reproduce individual bricks. The final site plans will be output as a scaled drawing from the CAD system. A further more generalised plan of the station within the context of the overall locality will be presented utilising an OS 1:10,000 map base.

### **3.3 PHOTOGRAPHIC SURVEY**

- 3.3.1 A general oblique photographic survey will be undertaken of the station buildings in accordance with the RCHM(E) Level 2 recording. The record would be fully indexed and photographic views would be shown with respect to the existing architects plans.
- 3.3.2 A conventional monochrome medium format record would be undertaken of the station buildings including internal, external elevations and appropriate architectural detail. A record would be made in 35mm colour print and black and white formats of the interiors of all room interiors and also the wrought iron-roof structure, showing similar detail to the medium format record as well as a broad range of generalised views.
- 3.3.3 The photographic record of the station buildings will include:
- i) General external coverage (black and white contact prints (35mm and medium format) and colour print
  - ii) General internal coverage (black and white contact prints and colour print (35mm)). This will include internal elevations of the rooms and also the second storey dormitory accommodation.
  - iii) Close-up views of representative samples of architectural details (black and white contact prints and colour print (35mm)).
  - iv) General views of representative structural detail (black and white contact prints and colour print (35mm)).
  - v) Detail views of the wrought iron roof structure (black and white contact prints and colour print (35mm)).
  - vi) Detail views will include the etched glass in the booking hall, and the large station (black and white contact prints and colour print (35mm)).

- 3.3.4 A rectified photographic record will be produced of the central section of a single canopy truss. The outline sections of the truss and other significant trusses will be recorded by oblique photography.

### 3.4 DESCRIPTION AND ANALYSIS

- 3.4.1 A visual inspection of the site will be carried out and a general descriptive record will be maintained of the structure utilising the appropriate LUAU pro forma record sheets to the Royal Commission on Historic Buildings in England (RCHME) Level 2 standard. It will involve the internal and external examination of the extant fabric, where health and safety allows, and will generate a description and assessment of the period and significance of the buildings. Where possible it will define the form and character of the building within a regional context. It will take particular note of elements relating to the dormitory accommodation on the second floor of the booking hall.

### 3.5 SURVEY REPORT

- 3.5.1 **Archive:** The results of the fieldwork will form the basis of a full archive to professional standards, in accordance with current English Heritage guidelines (*The Management of Archaeological Projects*, 2nd edition, 1991). The project archive represents the collation and indexing of all the data and material gathered during the course of the project. The deposition of a properly ordered and indexed project archive in an appropriate repository is considered an essential and integral element of all archaeological projects by the IFA in that organisation's Code of Conduct. This archive can be provided in the English Heritage Central Archaeology Service format, both as a printed document and on computer disks as ASCII files, and a synthesis (in the form of the index to the archive and the report) will be included in the Lancashire Sites and Monuments Record. A copy of the archive can also be made available for deposition with the National Archaeological Record. LUAU practice is to deposit the original record archive of projects (paper, magnetic and plastic media) with the appropriate County Record Office, and a full copy of the record archive (microform or microfiche) together with the material archive (artefacts, ecofacts, and samples) with an appropriate museum, in consultation with the County Museums Service.
- 3.5.2 **Survey report:** one bound and one unbound copy of a written synthetic report will be submitted to the Client, and a further copy submitted to the Lancashire County Council SMR. The report will include a copy of this project design, and indications of any agreed departure from that design. It will present, summarise, and interpret the results of the programme detailed above and present an assessment of the architectural significance of the structures on the site and will include photographs of any significant features. The report will also include a complete bibliography of sources from which data has been derived, and a list of further sources identified during the programme of work, but not examined in detail.
- 3.5.3 Illustrative material will include a location map, site map and building plans, and also pertinent photographs. It can be tailored to the specific requests of the client (eg particular scales etc), subject to discussion. The report will be in the same basic format as this project design; a copy of the report can be provided on 3.5" disk (IBM compatible format).

### 3.6 OTHER MATTERS

- 3.6.1 **Health and Safety:** LUAU conforms to all health and safety guidelines as contained in the Lancaster University Manual of Health and Safety and the safety manual compiled by the Standing Conference of Archaeological Unit Managers. The work will be in accordance with Health and Safety at Work Act (1974), the Council for British Archaeology Handbook No. 6, *Safety in Archaeological Fieldwork* (1989).
- 3.6.2 Full regard will, of course, be given to all Health and Safety considerations. LUAU provides a Health and Safety Statement for all projects and maintains a Unit Safety policy. A risk assessment will be completed in advance of the project's commencement.
- 3.6.3 **Confidentiality:** the report is designed as a document for the specific use of the Client, for the particular purpose as defined in the project design, and should be treated as such; it is not suitable for publication as an academic report, or otherwise, without amendment or revision. Any requirement to revise or reorder the material for submission or presentation to third parties beyond the project brief and project design, or for any other explicit purpose can be fulfilled, but will require separate discussion and funding.

### 3.7 PROJECT MONITORING

3.7.1 LUAU will consult with the client regarding access to the site. Whilst the work is undertaken for the client, the County Archaeologist will be kept fully informed of the work and its results. Any proposed changes to the project design will be agreed with Lancashire County Archaeological Service in consultation with the Client.

#### 4. WORK PROGRAMME

4.1 The following programme is proposed:

##### 4.2 *Desk-top Study*

A five day period is required to undertake the photographic survey.

##### 4.3 *Field Survey*

A four day period is required to undertake the site survey.

##### 4.4 *Prepare Evaluation report*

An eight day period will be required to complete this element.

4.5 LUAU can execute projects at short notice once an agreement has been signed with the client. A summary report will be produced immediately following the fieldwork and the more detailed survey report will follow three weeks after the field work.

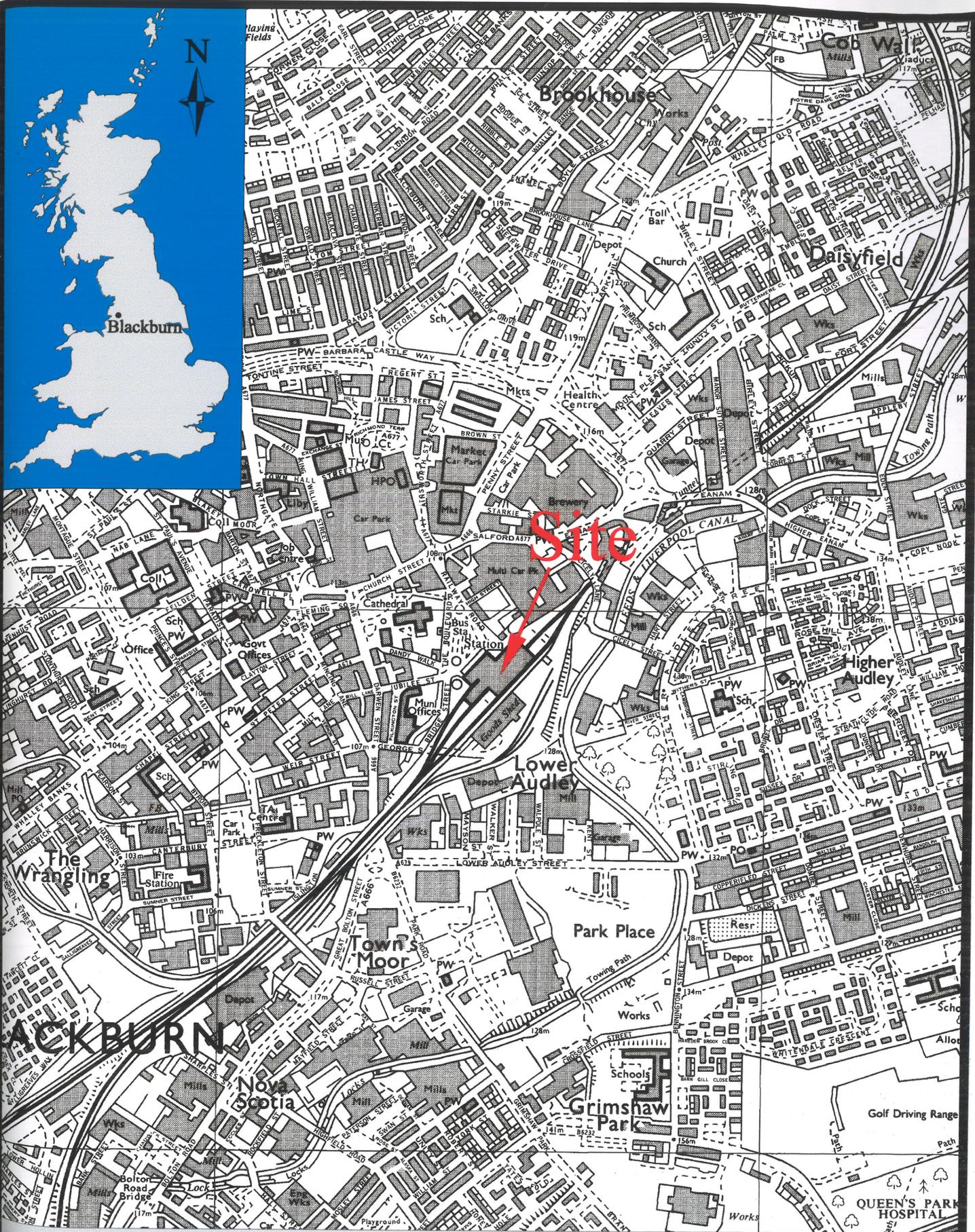
4.6 The project will be managed by **Jamie Quartermaine BA Surv Dip MIFA** (Unit Project Manager) to whom all correspondence should be addressed. LUAU adheres by the IFA's Code of Conduct and the Code of Approved Practice for the regulation of Contractual Arrangements in Field Archaeology.

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## ILLUSTRATIONS

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- Fig 1 Location Map
- Fig 2 Extract from 1848 6" Ordnance Survey map (surveyed 1846)
- Fig 3 Extract from 1894 25" Ordnance Survey map
- Fig 4 General Site Plan
- Fig 5 Station North End, Platform Level Plan - Buildings 2, 3, 4, 7, 8 and 9
- Fig 6 Station South End, Platform Level Plan - Buildings 1, 5 and 6
- Fig 7 Building 6, First Floor Plan
- Fig 8 Sub-Platform Plan
- Fig 9 Low Level Canopy (Platform 3) Truss - 1888 Design
- Fig 10 Column and Spandrel of the Low Level Canopy - 1888 Design



based upon the Ordnance Survey 1:10000  
 with the permission of the controller of HMSO  
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0 100 200 300  
 metres

Figure 1: Location Map



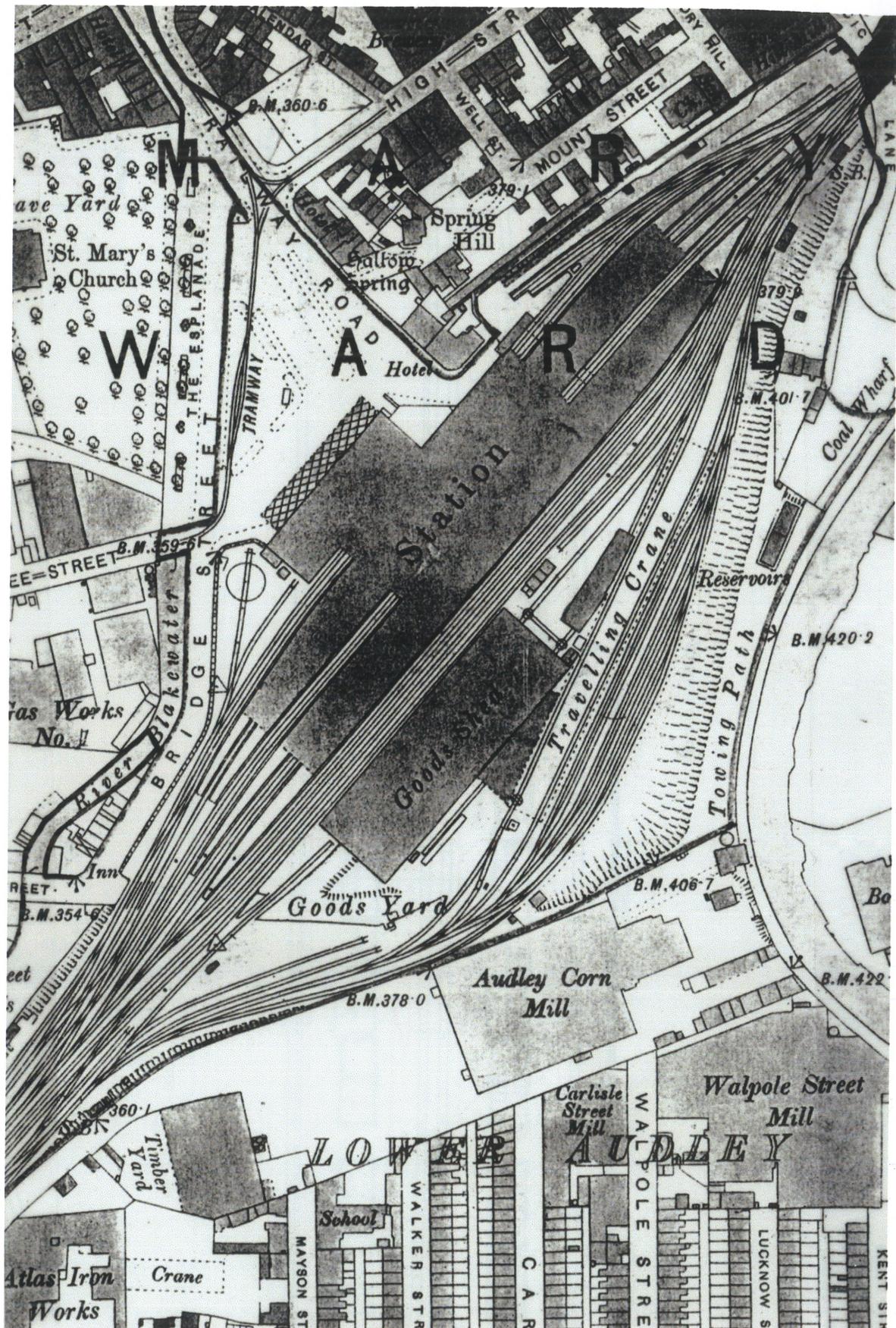


Fig 3 Extract from the 1894 25" Ordnance Survey Map

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TEL: 01524 840666



PROJECT:

Blackburn Station

DRAWING No:

4



DRAWN BY: M.T.

DATE: May 1999

KEY

8 Building Number



Underpass



Platform



Study Area

TITLE:

COMMISSIONED BY:

Oscar Faber

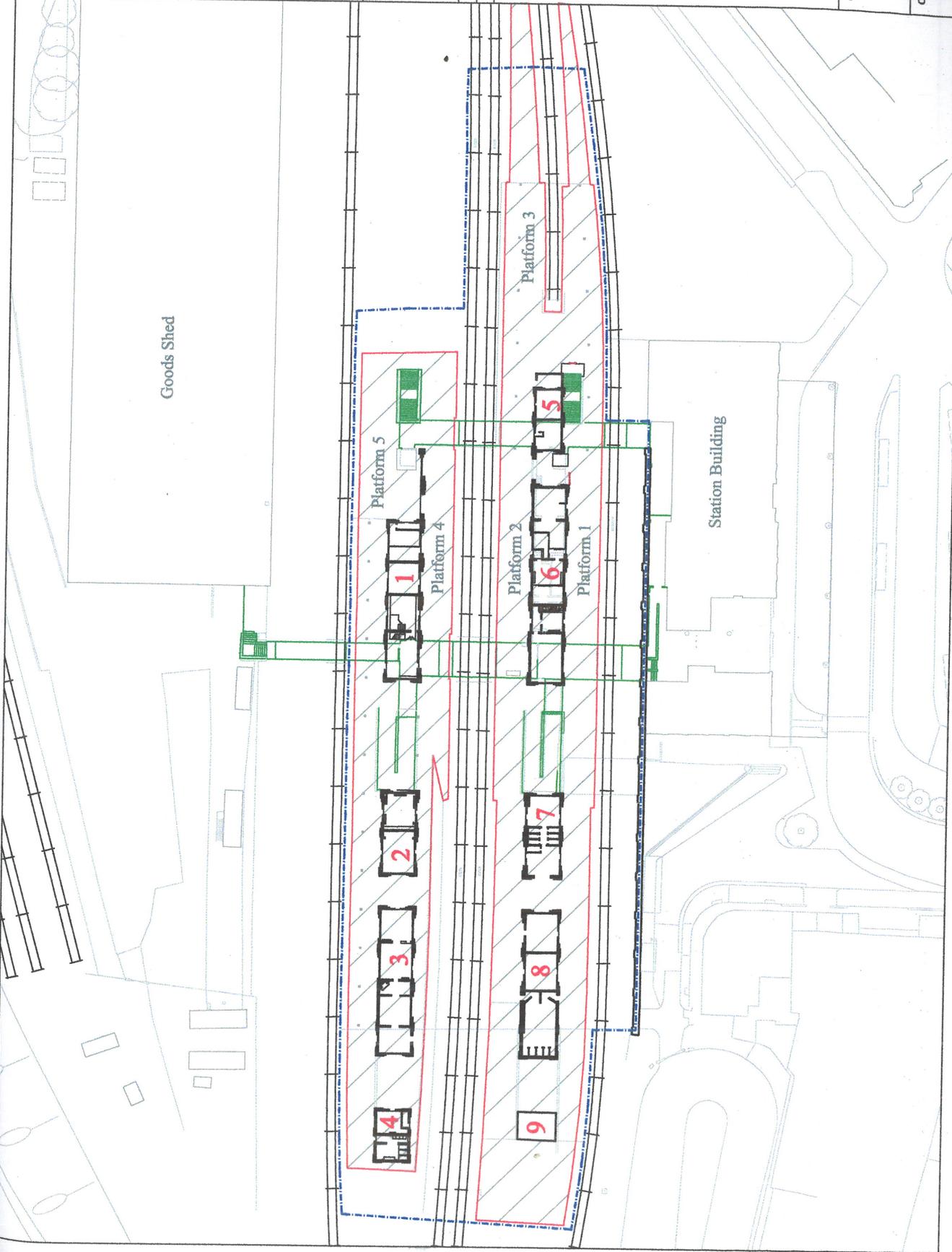


Fig 4: General Site Plan

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PROJECT:

Blackburn Station

DRAWING No:

5



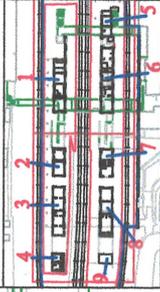
DRAWN BY: M.T.

DATE: May 1999

KEY

- 3 Building Number
- Underpass
- Platform
- Study Area

LOCATION



TITLE:

COMMISSIONED BY:

Oscar Faber

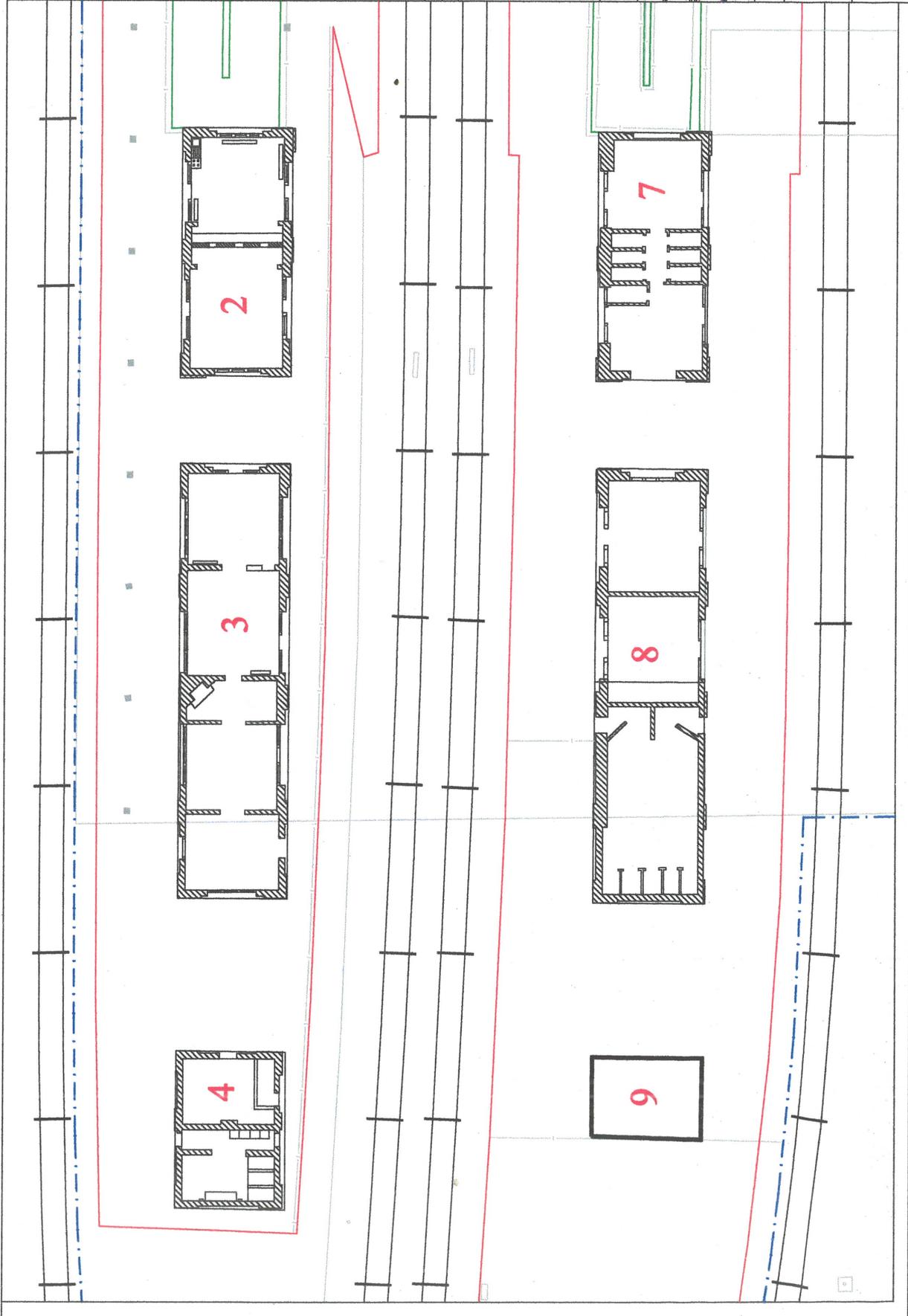


Fig 5: Station North End, Platform Level Plan - Buildings 2,3,4,7,8 and 9

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PROJECT:

Blackburn Station

DRAWING No:

6



DRAWN BY: M.T.

DATE: May 1999

KEY

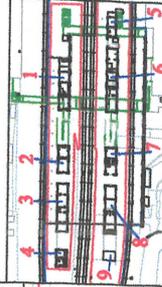
3 Building Number

Later Additions

Platform

Study Area

LOCATION



TITLE:

COMMISSIONED BY:

Oscar Faber



Fig 6: Station South End, Platform Level Plan - Buildings 1, 5 and 6

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PROJECT:

Blackburn Station

DRAWING No:

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DATE: May 1999

KEY

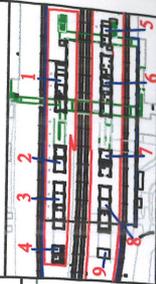
3 Building Number

Underpass

Platform

Study Area

LOCATION



TITLE:

COMMISSIONED BY:

Oscar Faber



Fig 7: Building 6, Second Floor Plan

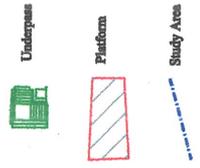
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PROJECT:  
**Blackburn Station**

DRAWING No:  
**8**



DRAWN BY: M.T.  
 DATE: May 1999  
 KEY



TITLE:

COMMISSIONED BY:  
**Oscar Faber**

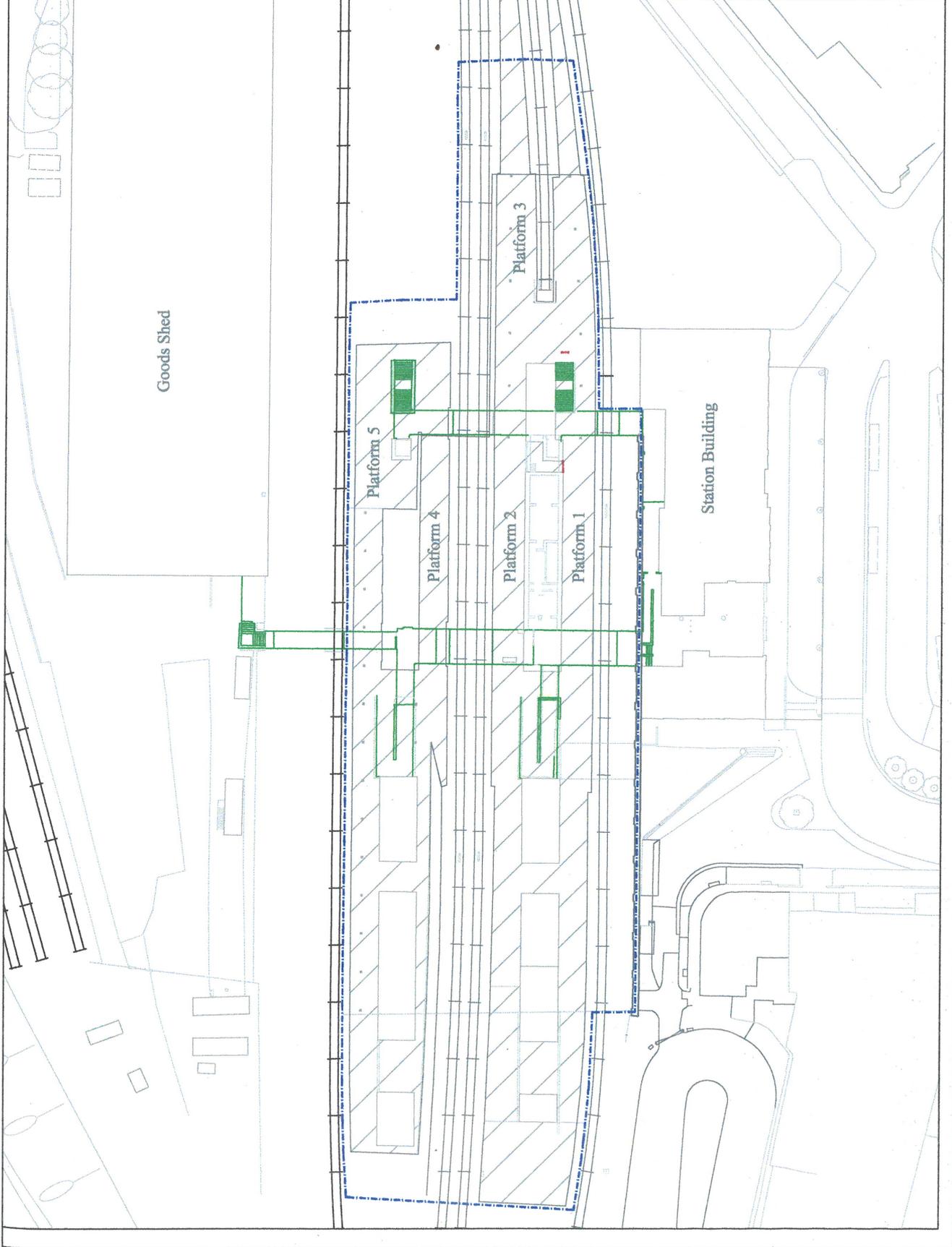


Fig 8: Sub-Platform Plan

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ARCHAEOLOGICAL UNIT  
LANCASTER  
LA1 1YW  
TEL: 01524 894666



PROJECT:  
BLACKBURN STATION  
BLACKBURN

DRAWING No: 9



DRAWN BY: A.S.  
DATE: March 1989  
REV

TITLE:  
Iron Truss on West Lower Level  
COMMISSIONED BY:  
OSCAR FABER

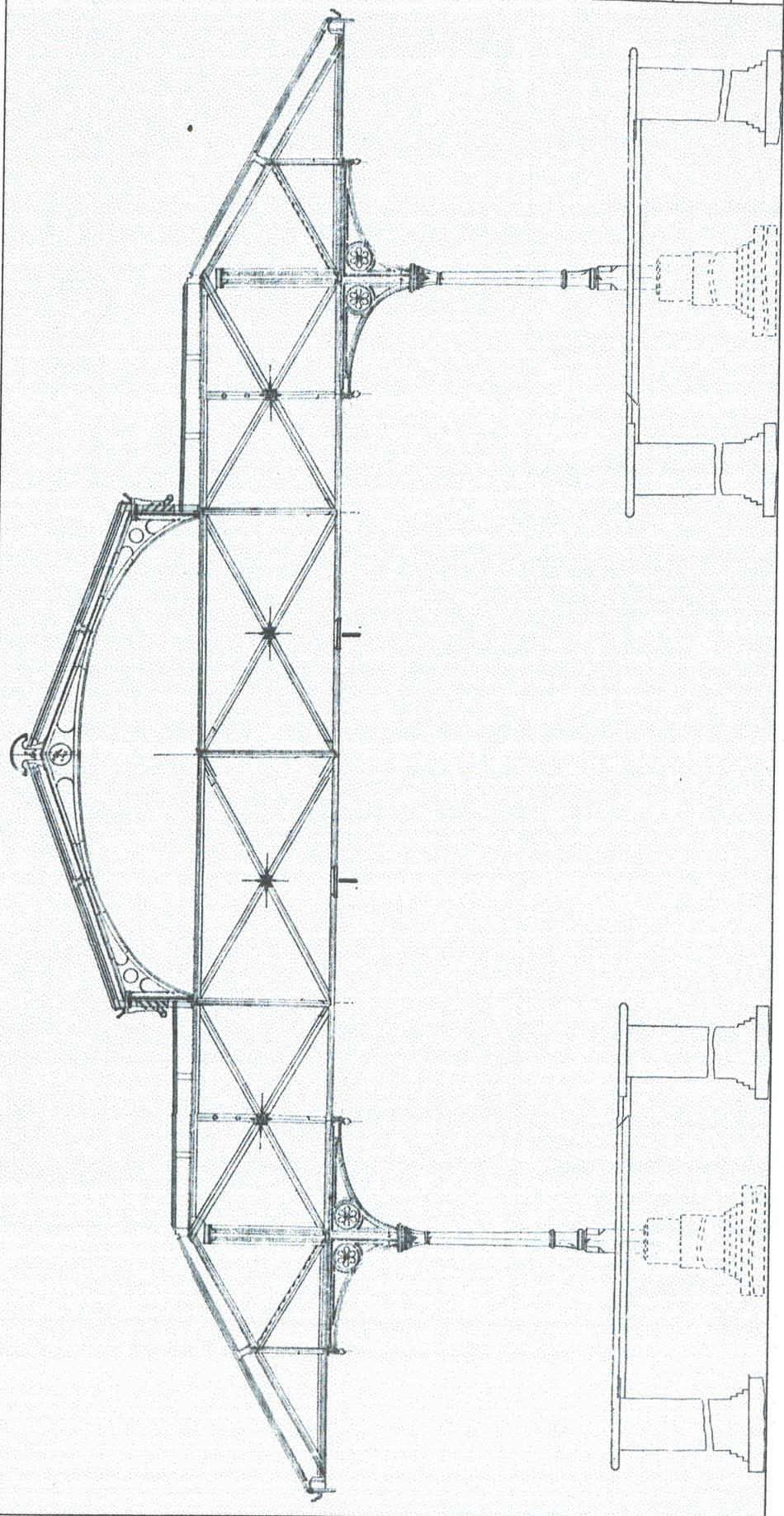
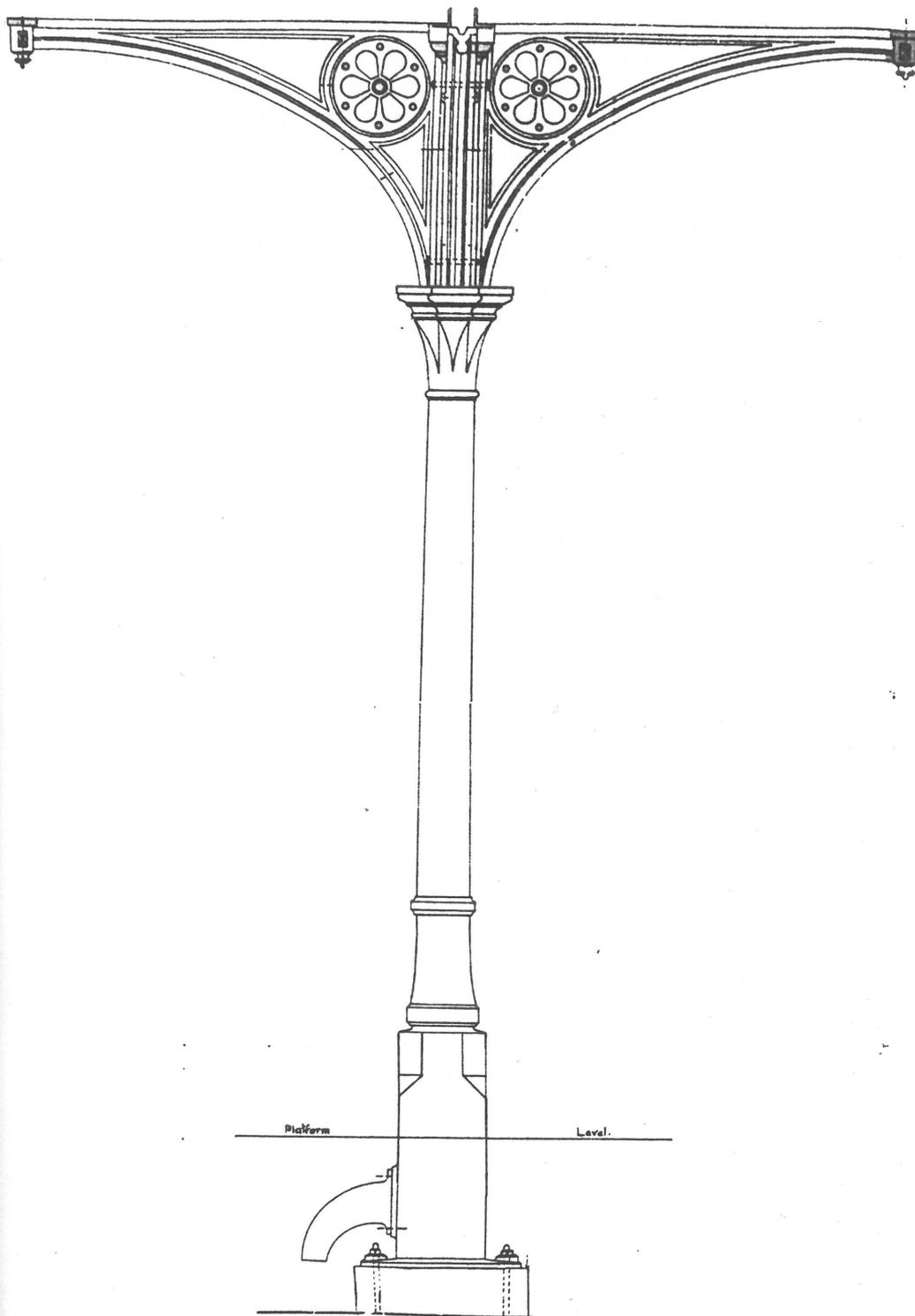


Fig 9: Low Level Canopy Truss over Platform 3 - 1888 Design Drawing



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LANCASTER  
LA1 1TF

TEL: 01524 848666

PROJECT:

Blackburn Station

DRAWING No:

10

0 2 0.4 0.6 0.8  
metres

0 0.5 1 1.5 2  
feet

DRAWN BY: JQ

DATE: May 1999

KEY

TITLE:  
Column and Spandrel of the  
Low Level Canopy

COMMISSIONED BY:

Oscar Faber

Fig 10 Column and Spandrel for the Low Level Canopy - 1888 Design Plan

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## PLATES

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- Plate 1 General view of Blackburn Station facade > SE
- Plate 2 South end of Platform 1, showing detail of the low level canopy truss
- Plate 3 General view of rear undergoing demolition
- Plate 4 Canopy clocks and original wrought-iron railing on gable end of Building 7
- Plate 5 Detail of clock on Platform 2
- Plate 6 Detail in dining room in Building 6
- Plate 7 Detail of etched glass in ladies 1st and 2nd class waiting room
- Plate 8 Internal view of building 3 > S
- Plate 9 Fireplace in Building 3
- Plate 10 Room 1, First Floor, Building 6: Dumb-Waiter in situ
- Plate 11 Dumb-waiter in situ, First Floor, Building 6
- Plate 12 Column support for low-level canopy



Plate 1: General shot of Blackburn Station Façade > SE



Plate 2: South end of Platform 1, showing detail of the low level canopy truss > NE

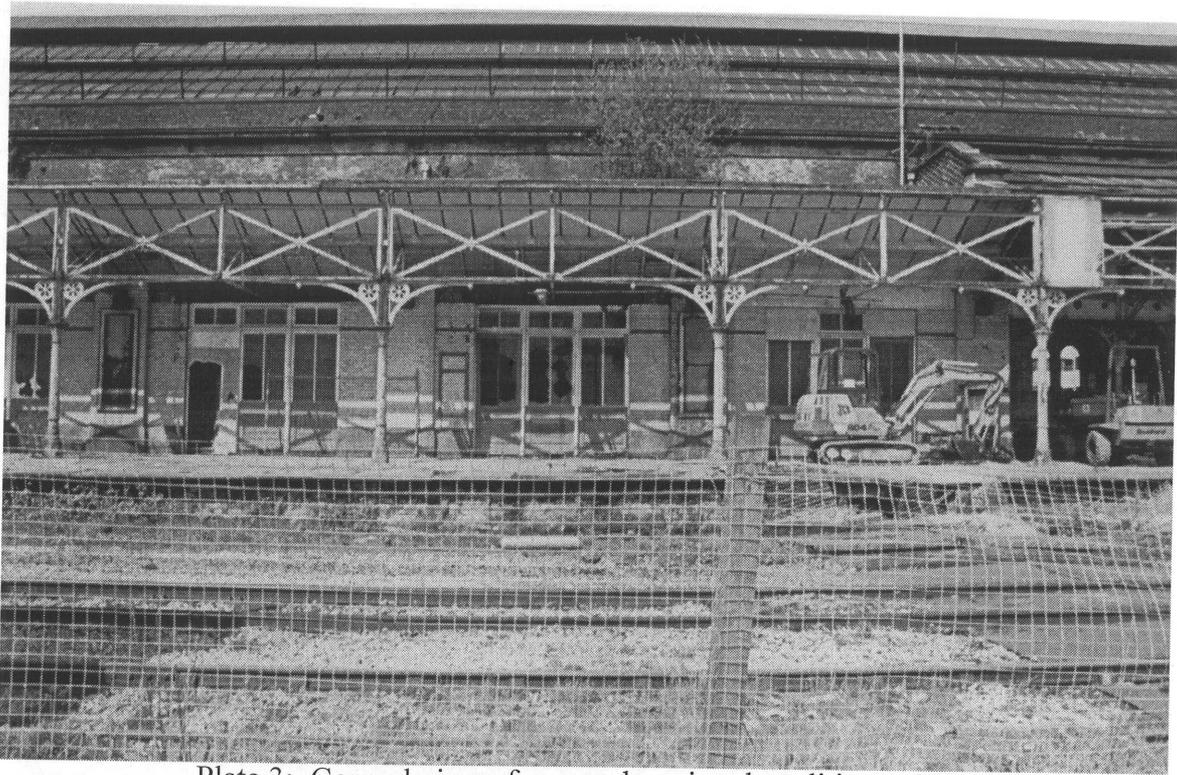


Plate 3: General view of rear undergoing demolition > NW

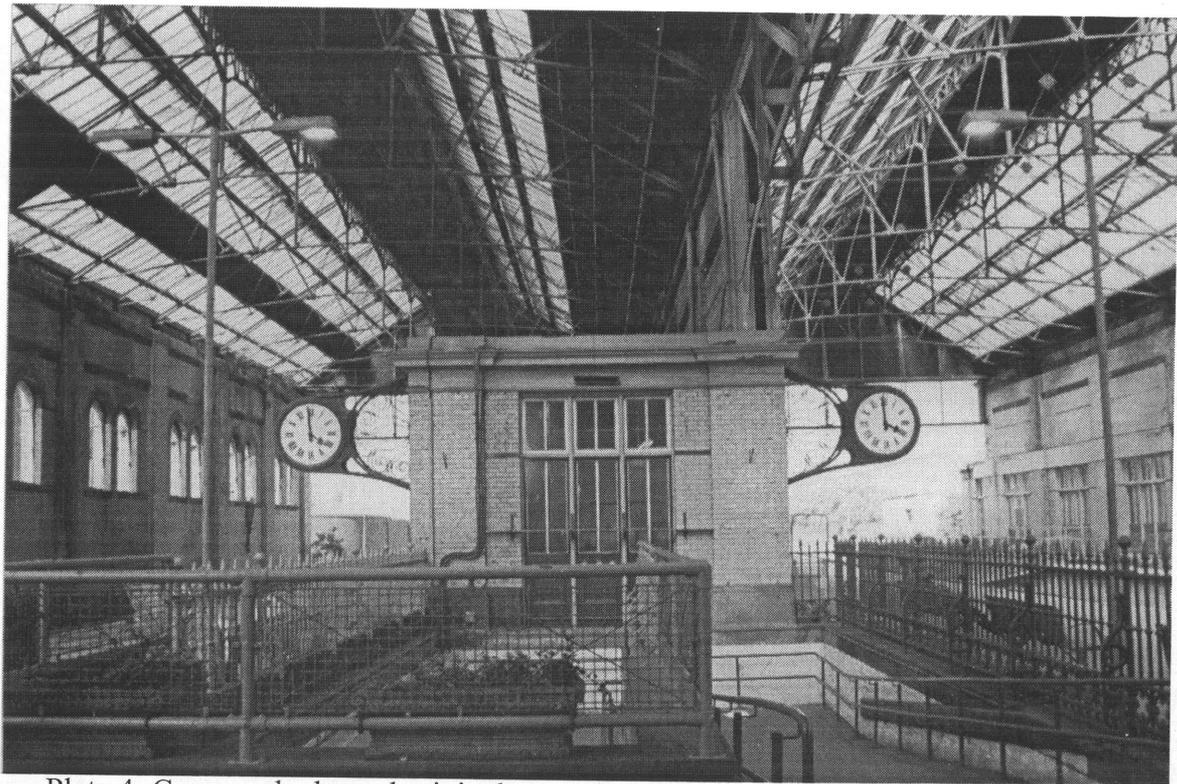


Plate 4: Canopy clocks and original wrought-iron railing on gable end of Building 7  
(Platforms 1 and 2) > NE

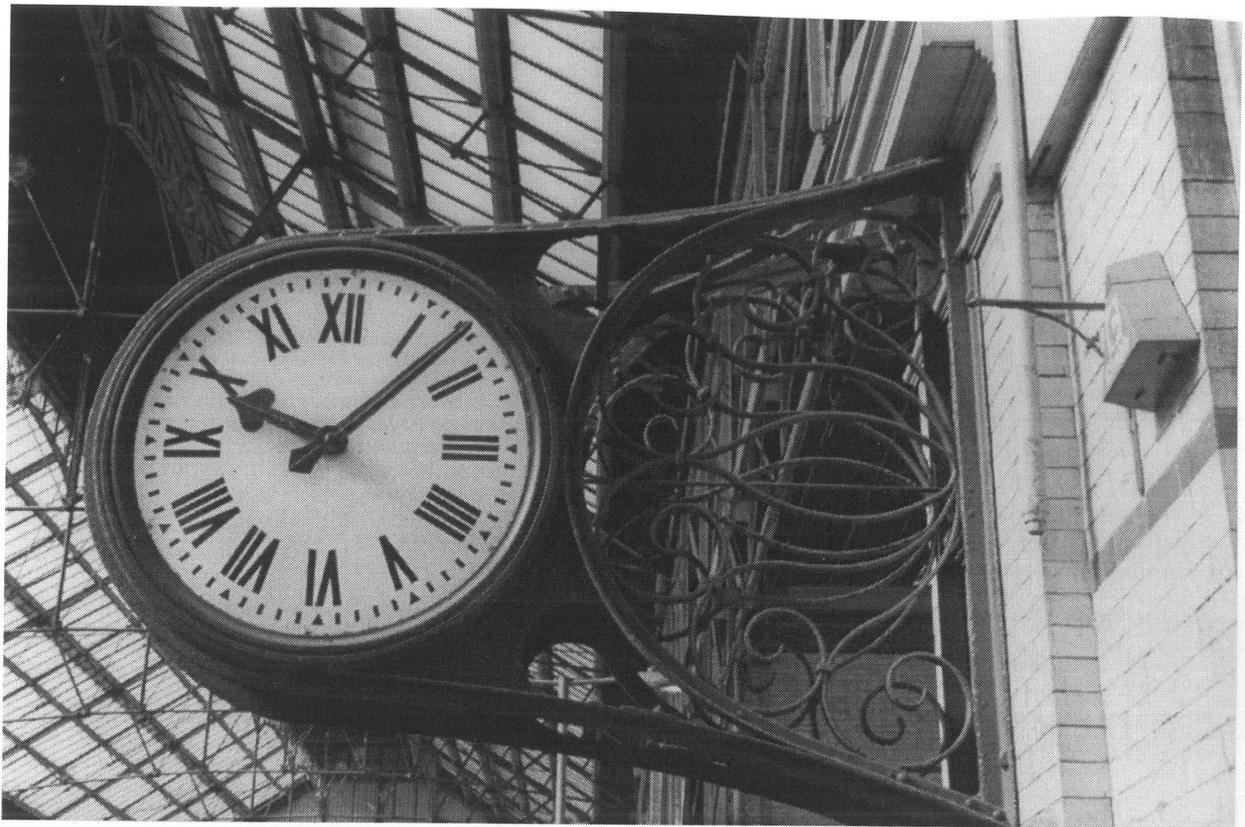


Plate 5: Detail of clock on Platform 2

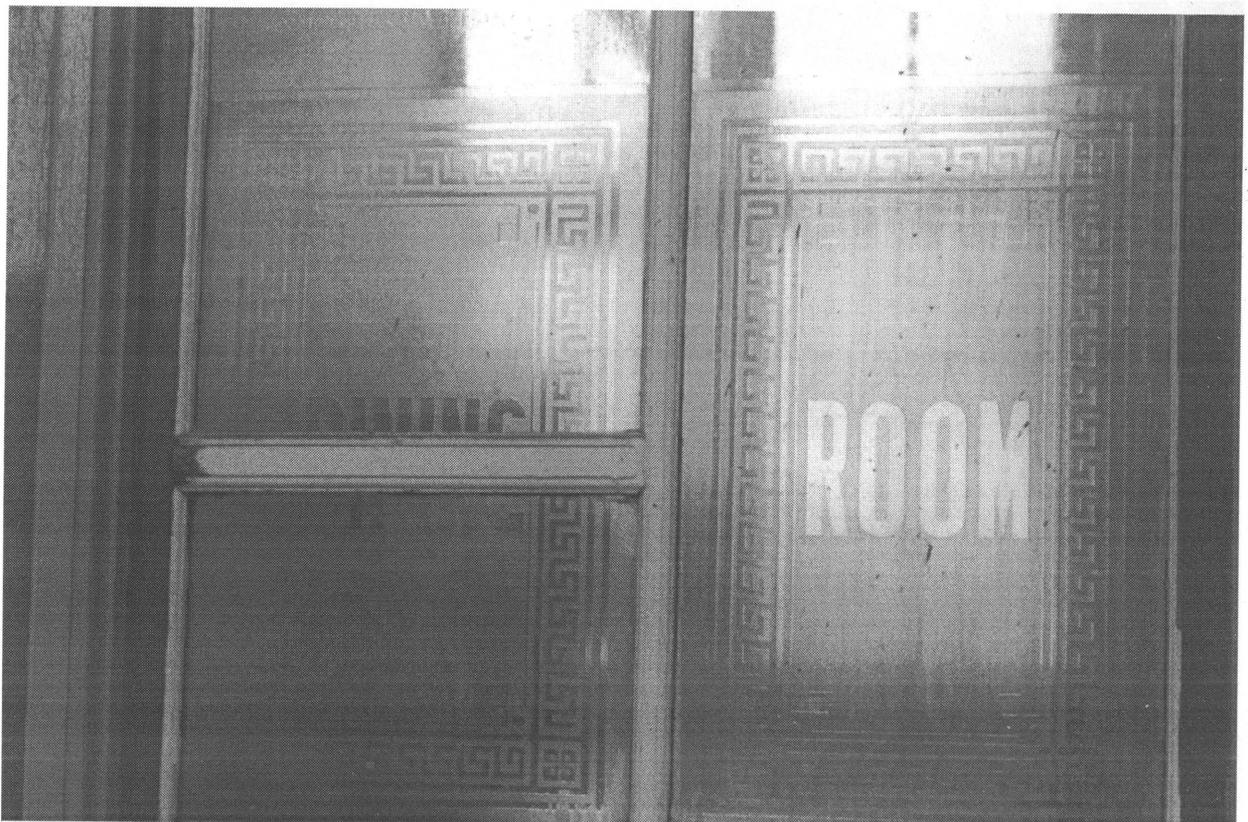


Plate 6: Detail in Dining Room in Building 6

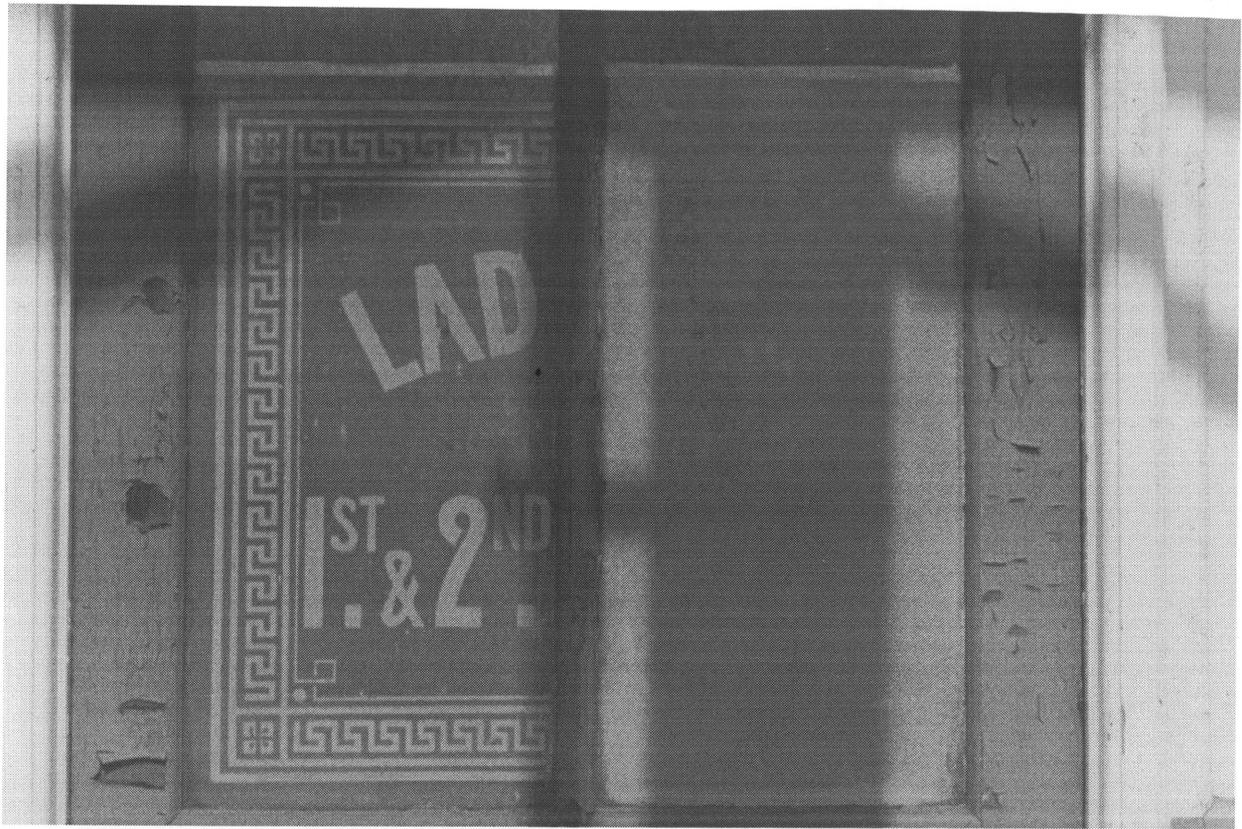


Plate 7: Detail of etched glass in Ladies 1<sup>st</sup> & 2<sup>nd</sup> Class Waiting Room



Plate 8: Internal view of Building 3 >S



Plate 9: Fireplace in Building 3

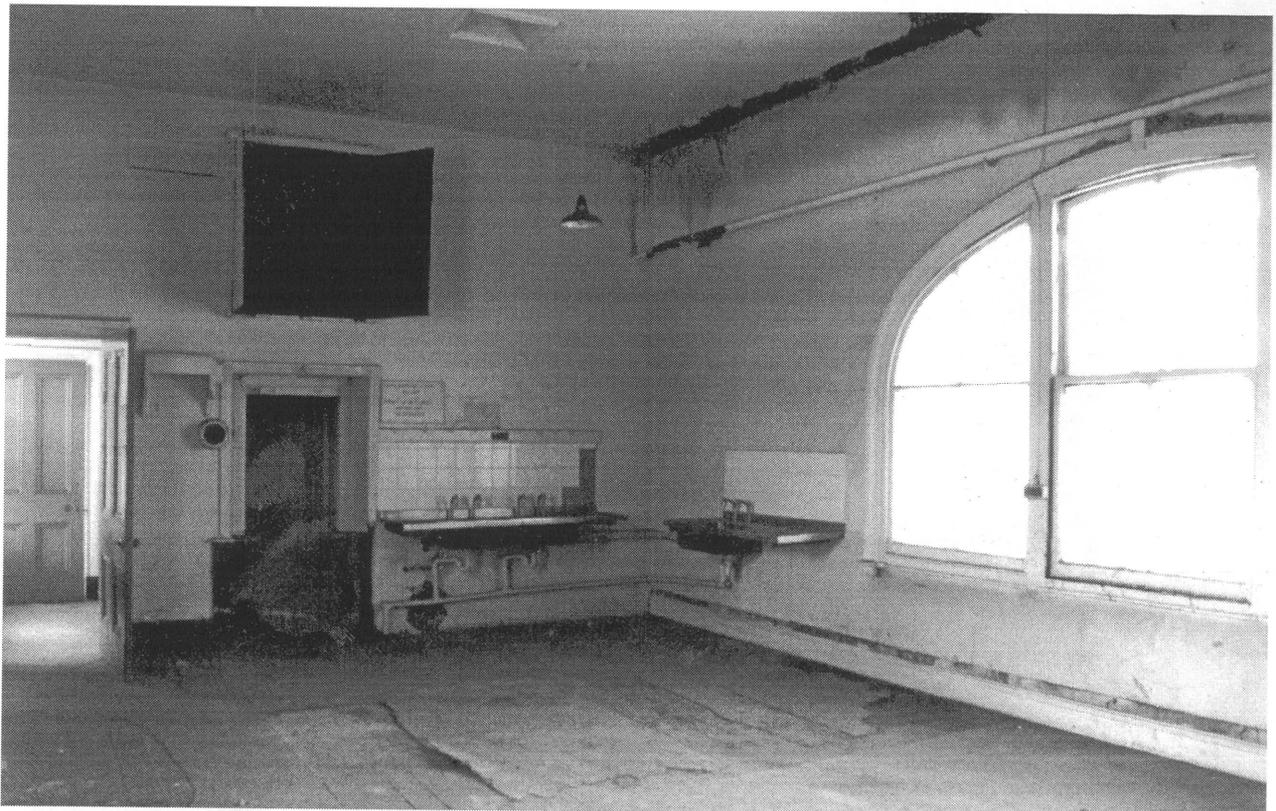


Plate 10: Room 1, First Floor, Building 6: Dumb-Waiter in situ

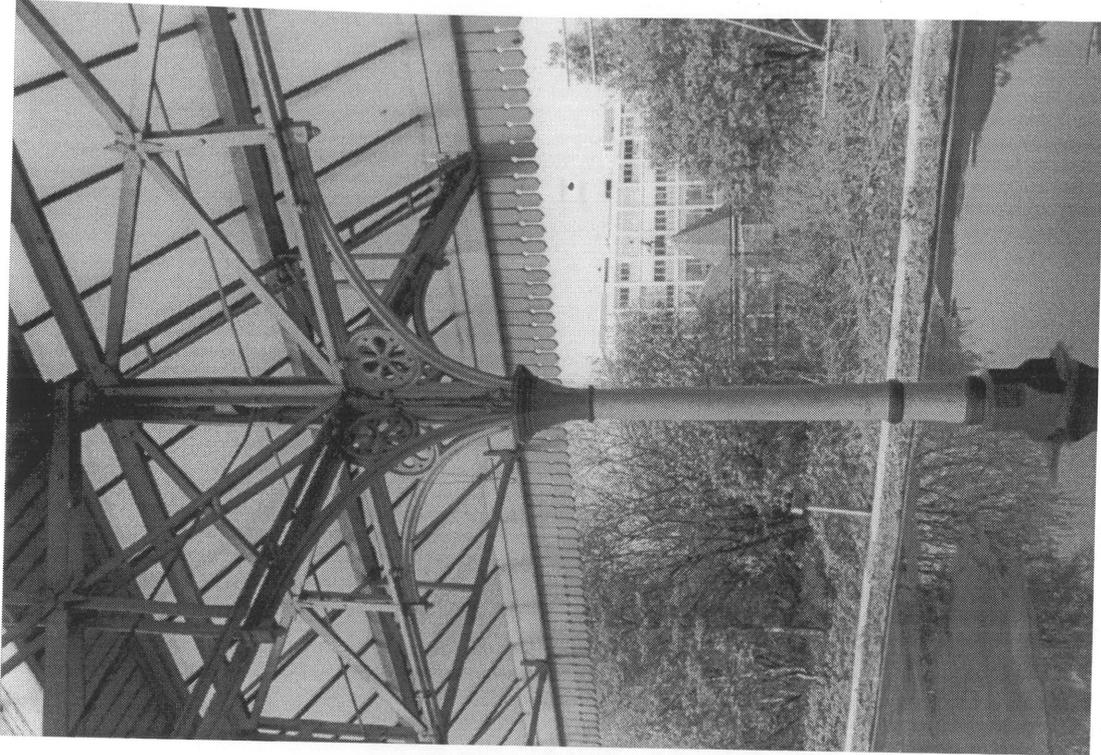


Plate 12 Column support for low-level canopy

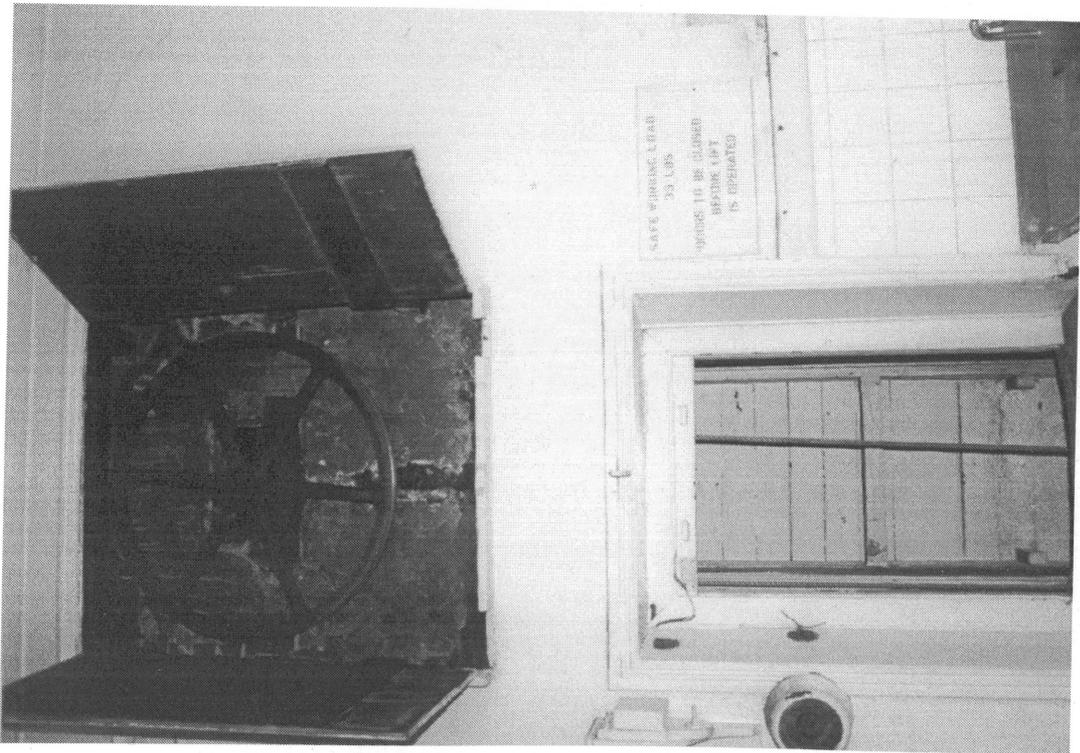


Plate 11 Dumb-waiter in situ, First Floor, Building 6