

Carbon Challenge Site  
Glebe Court  
Fletton  
Peterborough



**Archaeological  
Evaluation Report**



June 2012

**Client: CgMs Consultancy for  
Morris Homes Ltd**

OA East Report No: 1372

OASIS No:

NGR: TL 195 978

**An archaeological evaluation at Carbon Challenge Site, Glebe Court, Fletton,  
Peterborough**

*Archaeological Evaluation*

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*Report Date: June 2012*

**Report Number:** 1372  
**Site Name:** Carbon Challenge Site, Glebe Works, Glebe Court, Fletton, Peterborough  
**HER Event No:**  
**Date of Works:** 30th May - 1st June 2012  
**Client Name:** CgMs Consultancy for Morris Homes Ltd  
**Client Ref:**  
**Planning Ref:** 10/01267/FUL  
**Grid Ref:** TL 195 978  
**Site Code:** PETGLR12  
**Finance Code:** PETGLR12  
**Receiving Body:** Peterborough Museum

**Accession No:**

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Position: Project Officer  
Date: June 2012  
  
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Position: Senior Project Manager  
Date: June 2012  
Signed: .....



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## Summary

*Between 30th May and 1st June 2012 Oxford Archaeology East conducted an archaeological evaluation on land at Carbon Challenge Site, Glebe Court, Fletton, Peterborough in advance of a residential development (TL 195 978). Eleven evaluation trenches totalling nearly 300m in length were excavated across the southern part of a c.7.4ha site. These revealed modern deposits with the earliest feature comprising a ditch backfilled in c.AD 1800. Modern disturbances across the site related to its former use as a railway sidings and subsequent industrial use from the mid 20th century. No old ground surfaces were identified and the natural was truncated and directly overlain by modern layers.*



## 1 INTRODUCTION

### 1.1 Location and scope of work

- 1.1.1 An archaeological evaluation was conducted by OA East at Carbon Challenge Site, Glebe Court, Fletton, Peterborough (TL 195 978). This work was undertaken in accordance with a Specification prepared by CgMs (Flitcroft 2012) and carried out in line with a condition attached to planning consent.
- 1.1.2 The work was designed to assist in defining the character and extent of any archaeological remains within the proposed redevelopment area, in accordance with the guidelines set out in *National Planning Policy Framework* (Department for Communities and Local Government 2012). The results will enable decisions to be made by Peterborough Unitary Authority, with regard to the treatment of any archaeological remains found.
- 1.1.3 The site archive is currently held by OA East and will be deposited with the appropriate stores in due course.

### 1.2 Geology and topography

- 1.2.1 The complete development comprised a c.7.4ha area with the present evaluation located on the southern part of this, c.200m to the south of the River Nene (Figs 1 and 2). The ground level at the northern end of the site lies at 6.11m OD, rising slightly in the middle to 6.42m OD at the western end of Trench 4, then dropping to 4.97m OD near the southern boundary.
- 1.2.2 The British Geological Survey (BGS 1984) records two Drift geology deposits within the site comprising Alluvium in the northern side and 2nd River Terrace deposits within the southern part. These Drift geological deposits overlie Solid geology consisting of Oxford Clay.

### 1.3 Archaeological and historical background

- 1.3.1 The current archaeological work took place within the southern part of the development area (Fig. 1). This evaluation formed part of a series of archaeological works within the site which started with an assessment of the archaeological and historical background to the site (White 1998). An archaeological watching brief in 2000 during the mechanical excavation of test pits found two buried soil horizons (the first prehistoric and the second possibly post-Roman or medieval) near the River Nene directly to the north of the present evaluation (Redding 2000).
- 1.3.2 An evaluation took place in December 2003 in the north-western part of the development area (Hatton 2004) with the current site not evaluated at this stage as upstanding buildings were still present. Seven evaluation trenches were excavated and these identified probable post-medieval ditches which were likely to have been drainage channels feeding into the River Nene, as well as silted-up river channels. The report stated that 19th century railway and later 20th century industrial units may have removed some earlier remains (Hatton 2004).
- 1.3.3 Test pits were excavated in 2005 directly to the north of the present evaluation and recorded deposits relating to the 18th/19th century canal basin (Mackay 2005).



- 1.3.4 Nineteenth century railway sidings were located within the current site and were linked to Peterborough East railway station (part of the Great Eastern Railways/Eastern Counties Railway) directly to the north of the site. Wikipedia records that the Peterborough East station was originally the terminus for the line. It was opened on 2 June 1845, expanded in 1846 and closed to passenger traffic on 6 June 1966 and now only the engine sheds, goods shed and one platform remain. The station had services running west to Northampton as well as to the east to Wisbech and Norwich. After the railways were sold, the development area became an area of large industrial units with new buildings constructed which have recently been demolished.

## **1.4 Acknowledgements**

- 1.4.1 The author would like to thank Morris Homes and their consultants, CgMs, who commissioned the works. The project was managed by Richard Mortimer. The work was monitored by Dr Rebecca Casa-Hatton of Peterborough Unitary Authority. Fieldwork was carried out by the author, Gareth Rees (and survey) and Nicolas Cox. Dr Paul Spoerry commented on the pottery and the glass, Rachel Clarke edited the report and the Illustrations were drawn by Stuart Ladd.

## 2 AIMS AND METHODOLOGY

### 2.1 Aims

- 2.1.1 The objective of this evaluation was to determine as far as reasonably possible the presence/absence, location, nature, extent, date, quality, condition and significance of any surviving archaeological deposits within the development area.

### 2.2 Methodology

- 2.2.1 The Specification proposed that 14 evaluation trenches, each 30m long and at least 1.8m wide, were to be excavated within the development area (Flitcroft 2012). It was noted in the report that the north-western part of the site may be inaccessible due to concrete still remaining and therefore the four proposed trenches in this area may not be dug at this stage. During the monitoring of the evaluation it was decided by Dr Rebecca Casa-Hatton, the Peterborough City Archaeologist, that these trenches would not be excavated at this stage but a new trench would be placed next to the concrete to see how thick this deposit was in order to calculate whether it was likely that any archaeological deposits may have survived here. This was designed to ascertain whether the original proposed trenching here would be necessary once the concrete was removed. The locations of the other trenches were moved slightly to avoid haul road routes, hardcore and other mounds used by the developers.
- 2.2.2 Machine excavation was carried out under constant archaeological supervision with a wheeled 360°-type excavator using a 2m wide toothless ditching bucket.
- 2.2.3 The site survey was carried out by using a Leica GPS 1200. During the monitoring of the evaluation it was decided that the GPS could also be used to plan the exposed features. All archaeological features and deposits were recorded using OA East's *pro-forma* sheets. Photographs were taken of all relevant features and deposits. The evaluation took place in dry conditions. No environmental samples were taken.

### 3 RESULTS (FIG. 2; PLATES 1-4)

#### 3.1 Introduction

3.1.1 This section is described by trench. The evaluation only found modern features with the earliest being a ditch (1) backfilled in c. AD 1800. As a consequence, only the ditch was given context numbers whilst the later features were recorded in the overall trench description sheets.

#### 3.2 Trench 1

3.2.1 Trench 1 was 21m long and aligned north to south adjacent to the northern boundary of Zone C. It was shortened to allow access to a pile of chippings. In the northern 8m of the trench there were three service pipes (electricity, water and one unknown) running east to west across the trench. Three modern areas of disturbance were found in the remaining southern and central parts of the trench cutting the modern orange sandy alluvium. These modern features and the natural were sealed by a 0.25m thick black layer across the trench and this in turn was overlain by a 0.2m thick layer of hardcore.

#### 3.3 Trench 2

3.3.1 Trench 2, 28m long, was located in the south-western part of the site and was aligned roughly east to west. The natural, 0.85m below the ground level, comprised a mixed gravel and reddish brown clay sand. Cutting this natural deposit were five features consisting of a small pit containing modern 19th century brick, two drains, an electric service trench and a modern area of disturbance in the extreme western end of the trench. Three separate modern layers sealed these deposits.

#### 3.4 Trench 3

3.4.1 Trench 3 was in the north-eastern part of the site, it was 28.5m long and aligned north to south. Nine modern features were recorded cutting the natural orange sand and gravels. This included a group of seven similar pits within the southern part of the trench, probably all related. All were filled with a black deposit, some containing modern brick. The features and natural were sealed by two modern layers 0.4m thick.

#### 3.5 Trench 4

3.5.1 Trench 4 was within the middle of the site, it was 30m long and aligned east to west. Natural was encountered 0.3m below ground level in the middle and extreme western parts of the trench. In the eastern area, there was a 1.5m wide concrete raft with two areas of disturbance on either side of it. A large 6m wide disturbed area in the western half of the trench was fairly deep at more than 0.7m below ground level and contained concrete and stone pieces. One small isolated modern pit was found in the extreme western part of the trench.

#### 3.6 Trench 5

3.6.1 Trench 5 was in the south-eastern part of the site, it was 30m long and aligned north to south (Plate 3). Features were seen cutting the natural orange sands across the trench. In the southern part of the trench remains of part of a brick feature either square or rectangular in shape was identified that would have been an internal floor of a building. It comprised early perforated bricks laid in rows, with the bricks having two round holes located near either ends. Perforated bricks were being patented from the c.1830s by

Robert Beart of Godmanchester and manufactured at his Huntingdon brickworks. It is likely the bricks on the site date to the mid to late 19th century and this was presumably a small structure built for the railway sidings. In the middle of the trench there was a disturbed area over 7m wide with natural recorded at 0.9m below ground level. Modern features were also recorded at the far northern part of the site.

### **3.7 Trench 6**

3.7.1 Trench 6, placed at the far southern part of the site, was 31m long and aligned north to south. Features were seen cutting the natural between 0.57m (northern end) and 0.66m (southern end) below the ground level. In the middle of the trench, a single north-west to south-east aligned ditch (1) appears to have been backfilled c.AD 1800 (Plate 1). It was 0.76m wide and 0.21m deep with moderate sides and a concave base. The backfill (2) was a mid greyish brown silty clay. Within this deposit there was a small collection of artefacts with four pottery sherds (two brown stoneware sherds from a bowl, a manganese glaze on a white earthenware body and a cream ware sherd from a small plate (AD 1770+)), three undiagnostic brick fragments, part of a wine bottle (late 18th to early 19th century type), a piece of iron sheet and fragments of unidentifiable animal bone. There were also two areas of modern disturbance at the northern and southern parts of the trench as well as a modern service pipe which ran north-west to south-east across the southern-middle part of the trench and was backfilled with gravel.

### **3.8 Trench 7**

3.8.1 Trench 7 was located through a former Tarmac road, directly to the south of the existing concrete area. The trench was 27m long and aligned east to west. A single small pit containing 20th century glass cut the natural sands and gravels near the western baulk. Sealing the natural was hardcore, 0.3m thick and tarmac, 50mm thick.

### **3.9 Trench 8**

3.9.1 Trench 8 was positioned in the middle of the site, directly to the east of the concrete area, and was aligned north to south (Plate 2). A number of features were seen cutting the natural sands and gravels including what was probably part of the former 20th century railway line, within a 2m-wide area at the far northern extent of the trench. This comprised two parallel wooden sleeper planks, c.1.2m apart, inbetween which was a layer of crushed brick pieces within a black ashy deposit (Plate 2). There were also several small pits filled with a similar ashy deposit within different parts of the trench and these are also likely to have been related to the railway. An area of disturbance was located at the extreme southern extent which may have been either 19th or 20th century in date. These modern features were sealed by a 0.15 thick black layer which was in turn sealed by a 0.25m-0.3m thick deposit of hardcore.

### **3.10 Trench 9**

3.10.1 Trench 9, 30m long, was located within the middle of the site, it was aligned north-east to south-west. There were several modern features found across the trench. It is likely that these were associated with the railway or its demise, comprising two small pits filled with black ashy deposits (and modern brick) in the extreme northern extent as well as a feature in the southern side. This latter feature had parts of two wooden sleepers within its black ashy backfill. In the middle of the trench there was an area of modern disturbance located at least 0.8m below the ground level which had concrete in its

backfill. Two service trenches were also recorded. The features were sealed by two modern layers between 0.36m and 0.45m thick.

### **3.11 Trench 10**

3.11.1 Trench 10 was in the far northern part of the site to the east of the concrete area. Cutting natural orangey brown silts at 0.5m below ground level, were two modern features including a concrete block seen near the section at the north-western corner of the trench.

### **3.12 Trench 11**

3.12.1 A small trench, c.2.5m long and 1m wide, was located at the south-eastern corner of the concrete area (Plate 4). At the corner, the trench found a large concrete pillar still extending into the ground. Directly to the west there was a 0.1m thick area of disturbance sealing the natural gravel. This was overlain by a 0.35m thick concrete block which was in turn sealed by a 0.4m thick concrete slab.

## 4 DISCUSSION AND CONCLUSIONS

### 4.1 Overview

- 4.1.1 The evaluation uncovered widespread disturbance across the site with no pre-modern remains found. There were no old ground surfaces; natural was sealed by modern layers in all the evaluation trenches. This reduction in the height across the site took place after the site reverted from railway use and presumably occurred when large industrial structures were constructed.
- 4.1.2 Although the north-western corner of the site was not evaluated due to the presence of reinforced concrete in this location, the evidence suggests that there is only an extremely remote chance that pre-modern archaeological remains survive in this location. It is noticeable that all three trenches (7, 8 and 10) located near to the concrete on its southern and eastern sides found natural between 0.35m and 0.5m below ground level, whereas, Trench 11 located next to the concrete area found two large concrete rafts and 0.1m of disturbed soil collectively 0.85m thick.
- 4.1.3 No pre-18th century artefacts were recovered from the site and the earliest feature found was backfilled c. AD 1800 and comprised a truncated east to west aligned ditch backfilled with a small quantity of artefacts. There were several truncated features found which date to the railway use of the site, as well as to its subsequent industrial use.

### 4.2 Significance

- 4.2.1 The evaluation did not find significant archaeological remains.

### 4.3 Recommendations

- 4.3.1 Recommendations for any future work based upon this report will be made by the Peterborough Unitary Archaeology Office.

## APPENDIX A. BIBLIOGRAPHY

British Geological Survey (BGS) 1984 *Peterborough sheet 158 Solid and Drift Edition 1:50 000 Series*

Flitcroft, M., 2012 *Specification for archaeological trial trenching. Carbon Challenge Site, Glebe Works, Glebe Court, Fletton, Peterborough* CgMs report (unpublished)

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Mackay, D., 2005 *South Bank Development* Cambridge Archaeological Unit report (unpublished)

Redding, M., 2000 *An Archaeological Watching Brief at the South Bank Opportunity Area, New Fletton, Peterborough, Cambridgeshire*. Cambridge Archaeological Unit report 411 (unpublished)

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## APPENDIX B. OASIS REPORT FORM

All fields are required unless they are not applicable.

### Project Details

OASIS Number	Oxfordar3-128518			
Project Name	An archaeological evaluation at Carbon Challenge Site, Glebe Court, Fletton, Peterborough			
Project Dates (fieldwork)	Start	30-05-2012	Finish	01-06-2012
Previous Work (by OA East)	No		Future Work	Unknown

### Project Reference Codes

Site Code	PETGLR12	Planning App. No.	10/01267/FUL
HER No.		Related HER/OASIS No.	

### Type of Project/Techniques Used

Prompt	Planning condition
Development Type	Housing Estate

### Please select all techniques used:

<input type="checkbox"/> Aerial Photography - interpretation	<input type="checkbox"/> Grab-Sampling	<input type="checkbox"/> Remote Operated Vehicle Survey
<input type="checkbox"/> Aerial Photography - new	<input type="checkbox"/> Gravity-Core	<input checked="" type="checkbox"/> Sample Trenches
<input type="checkbox"/> Annotated Sketch	<input type="checkbox"/> Laser Scanning	<input type="checkbox"/> Survey/Recording Of Fabric/Structure
<input type="checkbox"/> Augering	<input type="checkbox"/> Measured Survey	<input type="checkbox"/> Targeted Trenches
<input type="checkbox"/> Dendrochronological Survey	<input type="checkbox"/> Metal Detectors	<input type="checkbox"/> Test Pits
<input type="checkbox"/> Documentary Search	<input type="checkbox"/> Phosphate Survey	<input type="checkbox"/> Topographic Survey
<input type="checkbox"/> Environmental Sampling	<input type="checkbox"/> Photogrammetric Survey	<input type="checkbox"/> Vibro-core
<input type="checkbox"/> Fieldwalking	<input type="checkbox"/> Photographic Survey	<input type="checkbox"/> Visual Inspection (Initial Site Visit)
<input type="checkbox"/> Geophysical Survey	<input type="checkbox"/> Rectified Photography	

### Monument Types/Significant Finds & Their Periods

List feature types using the [NMR Monument Type Thesaurus](#) and significant finds using the [MDA Object type Thesaurus](#) together with their respective periods. If no features/finds were found, please state "none".

Monument	Period	Object	Period
Ditch & railway feat	Post Medieval 1540 to 1901	Pottery glass brick	Post Medieval 1540 to 1901
Rail features + Ind	Modern 1901 to Present		Modern 1901 to Present
	Select period...		Select period...

### Project Location ,,

County	-	Site Address (including postcode if possible)	
District	Peterborough Unitary Auth	Carbon Challenge Site, Glebe Court, Fletton, Off London Rd Peterborough	
Parish	-		
HER	Peterborough		
Study Area	7.4ha	National Grid Reference	TL 195 978



### Project Originators

Organisation	OA EAST
Project Brief Originator	-
Project Design Originator	Myk Flitcroft CgMs Consultants
Project Manager	Richard Mortimer OA East
Supervisor	Rob Atkins OA East

### Project Archives

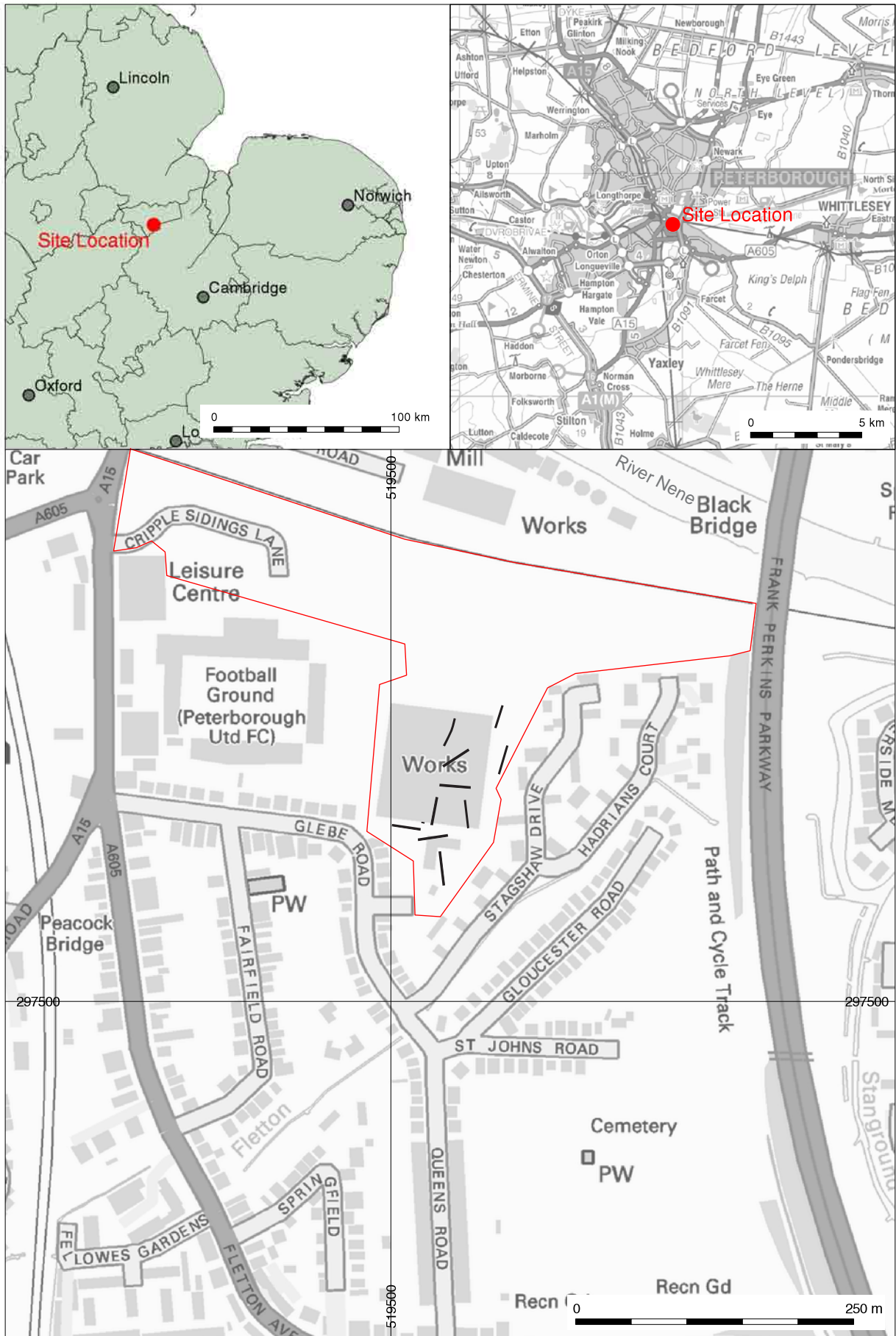
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OA East	OA East	OA East
PETGLR12	PETGLR12	PETGLR12

### Archive Contents/Media

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	<input type="checkbox"/> Plans
	<input checked="" type="checkbox"/> Report
	<input type="checkbox"/> Sections
	<input checked="" type="checkbox"/> Survey

### Notes:



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Figure 1: Site location showing development area and trenches

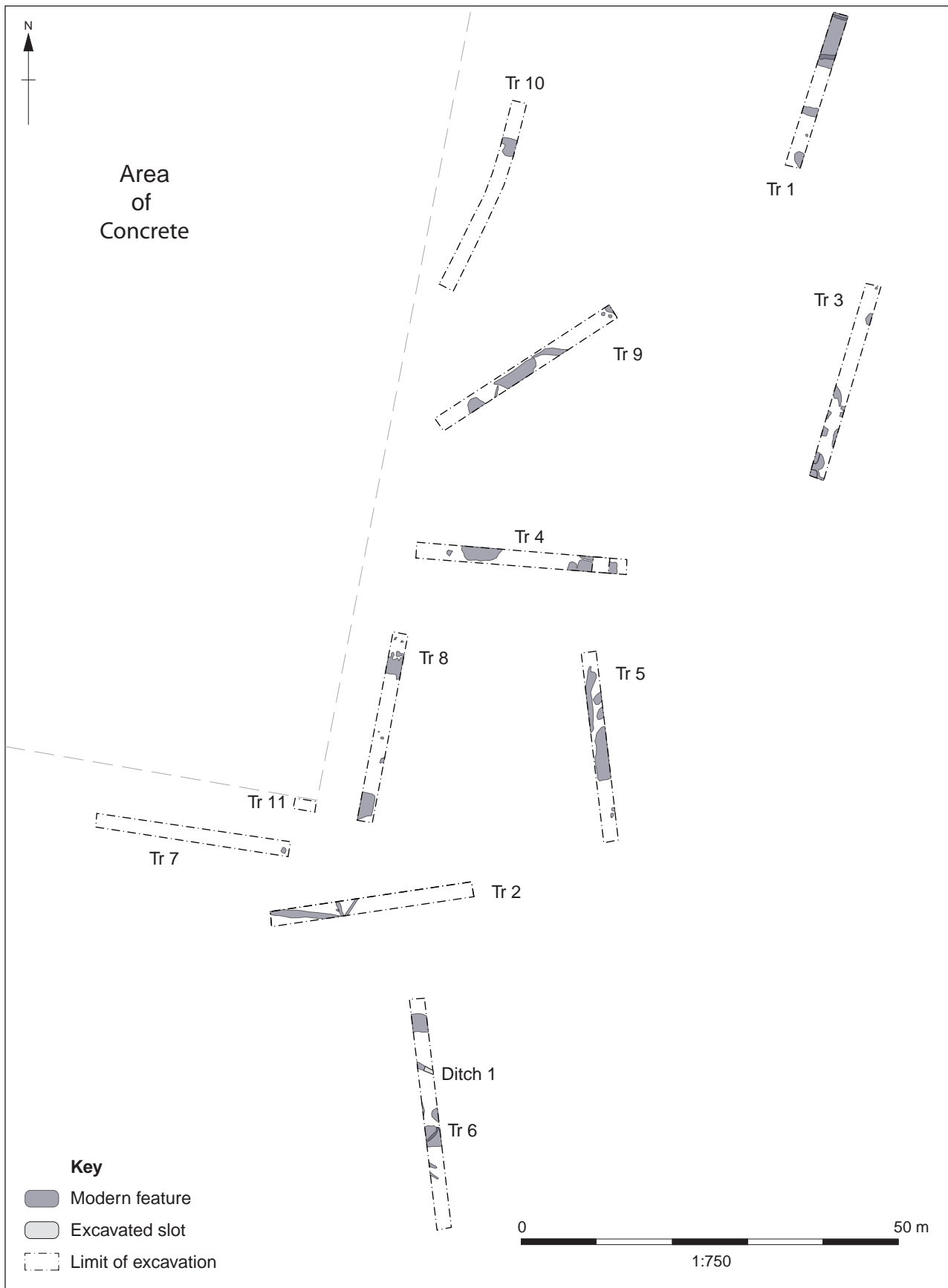


Figure 2: Trench layout showing modern features and deposits



Plate 1: Ditch 1 being excavated (Trench 6)



Plate 2: Trench 8 looking south with 'railway' feature within centre of trench



Plate 3: Trench 5 looking north showing feature comprising perforated bricks



Plate 4: Trench 11 looking north showing reinforced concrete rafts and concrete pillar



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